

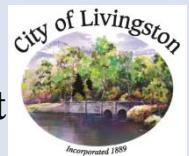
Transportation Alternatives Grant Program

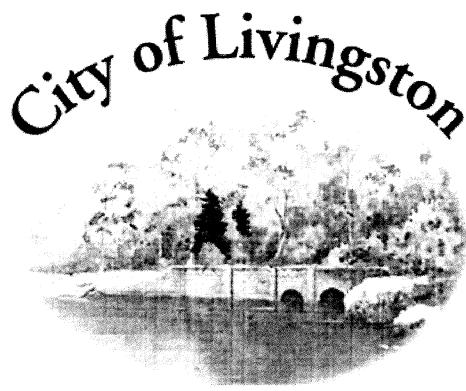
O-Street-to-Bennett Connector Project

*City of Livingston
March 2017*



Submitted by:
The City of Livingston
414 East Callender Street
Livingston, MT 59047



**Interim City Manager**

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March 7, 2017

Mr. Dave Holien, TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena, MT 59620-1001

Dear Mr. Holien and TA Review Committee,

On behalf of the City of Livingston, I am pleased to submit this Transportation Alternatives (TA) grant application for the proposed *O Street-to-Bennett Connector Project*. This proposed project represents an integral and safe connection for our active community and will clearly provide bicycle and pedestrian access to Livingston HealthCare and Myer's River Trail, as well as other facilities on the east side of Livingston. Notably, Livingston HealthCare is one of our leading employers with approximately 350 employees. The proposed project also aligns precisely with the Montana Department of Transportation's (MDT) eligible TA activities, as well as Federal and State transportation plans. Further, this project will assist MDT in meeting TA Bicycle and Pedestrian Transportation Policy Goals.

As you will see from the attached letters of support (Appendix H) and our local survey results (Appendix A), this project is firmly backed by local citizens, the adjacent property owners, city and county commissioners and other officials, the MDT Butte District, as well as multiple committees and coalitions. Moreover, this project represents a critical multi-modal safety connection for Livingston residents to access essential facilities, such as the hospital. The proposed *O Street-to-Bennett Connector Project* also connects to areas of planned growth and residential zoning annexations, businesses further east, and our extensive trails system. One could even safely use this relatively short corridor to eventually access County trails in the Gallatin National Forest.

The City places a great emphasis on developing and promoting safe routes for citizens to utilize, as evidenced by the City's past and upcoming infrastructure projects, partnerships with Park County, Livingston HealthCare, and community groups. This combined effort allows us to maintain existing trails, make area improvements, and develop multiple recreational trails and commuter paths for our community members. The proposed project is an important extension of these activities.

We thank you in advance for your consideration of our grant application. If you have any questions about the proposed project, please do not hesitate to contact me at 406-823-9870 or by email at llowy@livingstonmontana.org.

Sincerely,

Lisa L. Lowy Interim City Manager, Administrative Services and HR Director



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CITY OF LIVINGSTON GRANT PROPOSAL WEBSITE *Developed Internally to House Supplemental Material*

http://www.livingstonmontana.org/living/O-Street-to-Bennett_Connector_Project.html



CITY OF LIVINGSTON'S APPLICATION FOR A TRADITIONAL TA PROJECT

PROJECT NAME

O-Street-to-Bennett Connector Project – City of Livingston

LOCAL ENTITY SPONSOR AND POPULATION

*The City of Livingston
Population over 5,000 (2015 est. 7,302).¹*

PROJECT CONTACTS/ADMINISTRATIVE TEAM

Primary Contacts:

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Project Manager
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~or~

Lisa Lowy
Interim City Manager, Director of Administrative Services, and Human Resources Manager
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Livingston, MT 59047
(406) 823-9870
llowy@livingstonmontana.org

Secondary Contact:

Shannon Holmes
Director of Public Works
330 Bennett Street
Livingston, MT 59047
(406) 223-6565
sholmes@livingstonmontana.org

¹ <http://www.census.gov/quickfacts/table/PST045215/3043975>



ESTIMATED TOTAL PROJECT COST - \$283,788.21

	Transportation Alternative Program Fund	City of Livingston Cash Match	Total
Construction	\$152,639.52	\$23,659.30	\$176,298.82
Preliminary Engineering (30%)	\$45,791.86	\$7,097.79	\$52,889.65
Construction Administration (20%)	\$30,527.90	\$4,731.86	\$35,259.76
Indirect Costs (10.97%)	\$16,744.56	\$2,595.42	\$19,339.98
Project Total	\$245,703.84	*\$38,084.37	\$283,788.21

*The project is estimated to total \$283,788.21. Notably, 80.9% of the project cost and 0.45 miles fall within MDT's right of way (ROW), but we understand that MDT has recently incurred budget stresses. Given the importance of this project, we are willing to offer the full match amount contingent on MDT being unable to resolve a funding fix. Thus, should MDT's budget issue be resolved, we would ask that our match instead cover the customary portion of the project not located within MDT ROW (i.e., O Street Sidewalk; *\$7,263.73 match). We would also like to note that we calculated our budget estimate based on using a 10% construction contingency rather than 20% given that we have existing design plans in place, which we had prepared in 2015 by Stahly Engineering ([Appendix G](#)); we have updated these plans to include current construction cost estimates. Please see the **Budget Section** (pg. 12) for more details.

Cost Estimate Details

Engineer's Opinion of Probable Cost

#	Quantity	Item	Unit	Unit Cost	Total
101	1	Mobilization & Insurance	LS	\$ 16,000.00	\$ 16,000.00
102	1	Traffic Control	LS	\$ 8,500.00	\$ 8,500.00
103	1	Final Grading and Seeding	LS	\$ 3,500.00	\$ 3,500.00
104	1	Clearing/Grubbing/Tree Removal	LS	\$ 4,500.00	\$ 4,500.00
201	1725	Fill Material	CY	\$ 25.00	\$ 43,125.00
202	350	Strip and Replace Topsoil	CY	\$ 6.50	\$ 2,275.00
203	321	Crushed Base Course	CY	\$ 35.00	\$ 11,226.92
204	204	Asphalt	TN	\$ 137.50	\$ 28,081.73
301	1922	4 Inch White Paint	LF	\$ 4.00	\$ 7,688.00
302	4	Delineator	EA	\$ 250.00	\$ 1,000.00
303	1	Sign Post	EA	\$ 300.00	\$ 300.00
304	1	Remove/Replace Mailbox	EA	\$ 200.00	\$ 200.00
305	1	Monitoring Well Rim Adjust	EA	\$ 250.00	\$ 250.00
409	1980	4" Sidewalk	SF	\$ 8.00	\$ 15,840.00
410	330	6" Sidewalk	SF	\$ 12.00	\$ 3,960.00
411	175	HC Ramp Sidewalk Approaches	SF	\$ 18.00	\$ 3,150.00
412	5	HC Ramp Detectable Warning (Cast Iron)	EA	\$ 595.00	\$ 2,975.00
413	385	Concrete Driveway Approach	SF	\$ 20.00	\$ 7,700.00
	1	Contingency (10%)	%	\$ 16,027.17	\$ 16,027.17
Estimated Total: \$ 176,298.82					
Preliminary Engineering			30%		\$ 52,889.70
Construction Administration			20%		\$ 35,259.76
Right of Way and Utility Costs			0%		\$ 0
Indirect Costs			10.97%		\$ 19,339.93
Project Total					\$ 283,788.21

Shared-Use on Highway 89 S./Park

Sidewalk on O St.



PROJECT ELIGIBILITY

The *O-Street-to-Bennett Connector Project* aligns well with eligibility criteria under FAST Act, TranPlanMT, and local transportation plans.

Project Alignment with FAST Act

With respect to Eligible Projects under FAST Act (23 U.S.C. 133(h)(3)), the proposed *O Street-to-Bennett Connector Project* clearly meets Transportation Alternatives as defined in section 101 A and B. [i.e., **A.** “...on-road and off-road trail facilities for pedestrians, bicyclists, and other non-motorized forms of transportation, including sidewalks, bicycle infrastructure, pedestrian and bicycle signals, traffic calming techniques, lighting and other safety-related infrastructure, and transportation projects to achieve compliance with the Americans with Disabilities Act (ADA) of 1990;” and **B.** “...infrastructure-related projects and systems that will provide safe routes for non-drivers, including children, older adults, and individuals with disabilities to access daily needs.”]

Project Alignment with TranPlanMT

In addition to meeting FAST Act eligibility criteria, the proposed project is consistent with the priorities of TranPlanMT. Indirectly, this project supports all of the policy goals but most closely aligns with (1) Traveler Safety, (2) Bicycle and Pedestrian Transportation, and (3) Land-Use Planning, as discussed below.

Traveler Safety: The foremost *goals* of the *O-Street-to-Bennett Connector Project* are to avoid vehicle-pedestrian and vehicle-bicycle collisions, which clearly aligns with the goals in TranPlanMT. Further, this proposal aligns with safety issues identified by the public in Tran Plan 21 under Bicycle and Pedestrian Safety related to insufficient width for RVs and trucks to pass bicyclists. In addition to bicycle/pedestrian-vehicle collisions, the proposed project will serve to prevent vehicle-vehicle collisions should, as it stands, a driver need to move over, thus placing the driver into the opposing lane of traffic, which this project will prevent.

Bicycle and Pedestrian Safety: In accordance with TranPlanMT, the proposed project will provide mobility benefits of bicycle and pedestrian facilities, and “travel options for those who are unable or chose not to drive.” This is particularly important considering that the proposed shared-use path will provide the *only* safe non-motorized access to Livingston HealthCare. Also in accordance with Montana’s transportation policies, this project will provide “infrastructure that promotes modal alternatives.” In addition to accessing business establishments and healthcare, this connection will provide access to the Myer’s River View Trail System along the Yellowstone River (See *Trail Connections Map* – [Appendix D](#)). The proposed project will also overcome system discontinuity through connecting to the multi-use path on MDT’s new Veteran’s Bridge.

Land-Use Planning: The *O-Street-to-Bennett Connector Project* is in agreement with the TranPlan 21 goal of “encouraging local land use planning with the additional specific focus of encouraging local jurisdictions to better support MDT’s corridor preservation objectives.” In the last decade, Livingston has grown in the direction of the proposed project. Further, from a planning standpoint, this is the most viable direction for future growth. Multi-modal transportation will enhance safely accessing existing and new developments, and this project will provide a safe transportation alternative for individuals to access this area from the Highway 89 South corridor (please see *Annexation Map* – [Appendix C](#) and Livingston City Planning Board’s Letter of Support, [Appendix H, pg. 43](#)).

Project Alignment with Local Transportation Plans

In addition to State and Federal goals, the *O-Street-to-Bennett Connector Project* aligns well with several needs identified in our local transportation plans, including both Livingston’s and Park County’s Trail Plans. Several years ago, this project was identified by our Parks and Trails committee as a top funding priority. The opportunity to bring this connection to fruition also addresses goals in our Parks and Trails Master Plan and the Growth Policy. Further, the City Planning Board has asserted that multi-modal transportation systems, pedestrian facilities, and bicycle paths are an integral part of their land-use decisions and recommendations.

Please note, if you wish to view our local transportation plans in more detail, please visit our webpage for the *O Street-to-Bennett Connector Project* at http://www.livingstonmontana.org/living/O-Street-to-Bennett_Connector_Project.html



PROJECT STATEMENT

a) Project Narrative/Abstract: The City of Livingston, as the eligible project sponsor, is requesting TA grant funds for the construction of an AASHTO-compliant shared-use path to connect our community and overcome a known safety concern on our busiest street. The *goal* of the proposed *O-Street-to-Bennett Connector Project* is to provide safe passage for bicyclists and pedestrians to critical facilities on Livingston's east side. This project includes the following two phases: (1) widening the shoulder on what is now a particularly narrow section of US Highway 89 South/Park Street/I-90 exit 337, and (2) the addition of an ADA-compliant pedestrian crossing and sidewalk on the east side of one block of O Street, which will provide a pedestrian corridor to the less-traveled Callender street (*see visual below and the Map of the Area of Interest on the next page*). It is important to note here that during preliminary design and discussions with community members, a separated path was also considered; however, the geography of this area/steep embankment prohibits this approach (See ADA Section). Fortunately, the alternative shared-use side path has the support of our MDT District (*see Appendix H, pg. 42*).



The proposed project's 0.45 mile stretch of Highway 89 South will extend from O Street, on the far east side of Livingston, to connect to an existing shared-use path that starts just before Veteran's Bridge, completed by MDT in 2013. Presently, there are portions east of Veteran's Bridge where the shoulder is less than 12 inches, with sharp drop-offs (*see above image and Letters of Support, Appendix H*). The proposed shared-use path will improve safety and offer a vital connection to the new Livingston HealthCare facility, the multi-use path on Veteran's Bridge, Myer's River View Trail, and other facilities (*see Letters of Support [Appendix H] and Connectivity Section, pg. 11*). Through overcoming known safety issues, individuals will be able to safely walk or ride a bicycle to critical facilities. We aim to further improve safety by generating a pedestrian and bicycle corridor to and from this connection through the addition of a pedestrian crossing on O Street, a sidewalk, and signage to help divert pedestrian and bike traffic off US Highway 89 upon reaching O Street.

The proposed *O-Street-to-Bennett Connector Project* is located within Livingston City Limits and three miles of our geographical center (*see Appendix E: Map of Geographical Center of Livingston*). **The shared-use path itself will fall entirely within MDT's 100-foot ROW**, and as such, we have proactively and collaboratively communicated with MDT regarding this area of concern (*see Appendix H: Letter of Support from MDT, pg. 42*). Moreover, in 2015, we contracted Stahly Engineering to create a preliminary design plan (*see Appendix G*), which cost the City over \$5,000 but helped reassure the feasibility of this project, that the fill slopes are within MDT's ROW, and budget accuracy. It is important to note that while it may appear in the design plans that the fill will exceed MDT's ROW, there are encroachment issues that this project will also resolve. As discussed in **MDT's letter of support**, “*the proposed facility meets criteria outlined in recent MDT policies (HSSRA and Shared-Use Path), including serving a transportation purpose (as evidenced by existing usage and user survey), being within a city or town, and enhancing transportation safety. The project additionally presents the District with opportunities to resolve existing encroachments with the benefit of local agency support and to improve the facility's side-slopes and, hence, overall maintainability.*”

Overall, the proposed connection will improve safety and connectivity by providing a shared-use path and pedestrian corridor to connect Livingston businesses on the east end to Livingston HealthCare and potential future developments (*see Appendix C, Map of Annexations and Growth Areas and Livingston City Planning Boards Letter of Support, Appendix H, pg. 43*), as well as provide a connection to existing trail systems east of Veteran's Bridge (*see Appendix D: Map of Trail Connections East and West*). Importantly, we have the means and equipment to successfully maintain this critical connector once constructed and will implement a signed Maintenance Agreement, as discussed on pg. 15. TA Program funds will allow us to serve our community and connect this missing link, making Livingston a safer place to walk and bike as transportation alternatives.



Legend

— O_street

— Narrow Shoulder with Steep Embankment

Proposed O Street Trail

0 0.025 0.05 0.1 0.15 0.2 Miles



Project Benefits – Safety, Accessibility, and Connectivity

SAFETY: The proposed *O Street-to-Bennett Connector Project* was conceived to overcome a serious safety concern involving one of the arguably busiest and most constricted travel corridor in Livingston, and perhaps in the entirety of Park County. Currently there exists a small stretch of Highway 89 South on the east side of Livingston that has quite narrow shoulders and drop offs; at some points, the slope is approximately 45° with a drop off of about 20 feet. Unfortunately, this 0.45 mile stretch is the only means for bicyclists and pedestrians to access the new location of our critical access hospital, Livingston HealthCare, which opened its doors in October 2015. Further, the new Veteran's Bridge on Highway 89 South, which crosses the Yellowstone River and is just east of the proposed the *O-Street-to-Bennett Connector Project*, was installed in 2013 and includes a shared-use path. However, this path is disjointed from the community due to the unsafe stretch. Veteran's Bridge also connects to Myer's River View Trail, which too provides access to the Yellowstone River, but its trailhead is also disconnected due to this narrow stretch. The proposed project will thus provide safe passage for bicyclists and pedestrians from Veteran's Bridge to O Street, which will extend to a pedestrian and bicycle corridor to and from this connection. Further, this project includes the addition of a pedestrian crossing on O Street, a sidewalk, and signage to help divert pedestrian and bike traffic off US Highway 89 and onto the less-trafficked Callender Street, upon reaching O Street ([Appendix B](#)), thus further overcoming safety concerns.

In addition to the narrow shoulder that limits safe passage, there are other issues factoring into safety risks along this key connection, including the following:

- This corridor serves as access to our **City's transfer station**, where vehicles commonly carry wide loads of construction materials, yard waste, and other hazards extending outside of their vehicles.
- This corridor is an **interstate entrance and exit (337)**, increasing the number of wide loads and commercial vehicles.
- Livingston is a popular tourist destination due to its proximity to national forests, wild lands, and Yellowstone National Park, leading to an increase in **RVs**, which have decreased visibility and their own special hazards.
- Due to the formidable winds in Livingston that can blow commercial vehicles over (**Figure 1**), Highway 89 is a **common reroute for I-90** drivers (**Figure 2**). The average daily traffic on I-90 through Park County/Livingston is 14,541 vehicles per day. Approximately 75 times out of the year or more, freight traffic and vehicles are recommended to take the I-90 detour.



Figure 1: Freight truck blown over on I-90 bridge over the Yellowstone River through Livingston.



Figure 2: I-90 Traffic re-routed through Livingston and adjacent to the proposed project location.

As you see, a number of serious safety concerns exist for this narrow stretch of Park Street that prevent or inhibit individuals from using alternative transportation to access the hospital, bridge overlook, and eastside trails. Those who do brave this stretch, perhaps due to a lack of other means, dedication to active transportation, or otherwise, place themselves and others at risk of serious injury or mortality, as evidenced in the following testimonials:

- “*Even as a seasoned bike racer and longtime bike commuter, I find the current commuting options [at this location] grim and dangerous.*” ~ Jimmy Udall
- “*Imagine your choice as you make your way along the busiest Highway route through the City ... 1) You go on 2 feet of pavement where on the left a five inch white line “protects” you from city traffic zooming by, or 2) You head down the steep 45 degree or so embankment for 15 yards and beat your way through weeds, deer remains, beer cans and other garbage before you “sneak” across some private driveways.*” ~ Bob Jovick
- “*I have ridden this path in its current conditions many times, both on my way to and from work...as traffic increased over the summer and the days shortened this fall, it became too dangerous to continue.*” ~ Dr. Laura Desnick, MD.

Fortunately, the project proposed *O-Street-to-Bennett Connector Project* will improve public safety and promote active transportation, all the while meeting TA funding criteria.



ACCESSIBILITY AND MEETING ADA

As discussed in the above Safety Section, even though this portion of Highway 89 South/Park Street serves as a vital connection to critical facilities, including our only hospital and Emergency Room, this section of road requires the addition of features to be in compliance with the Americans with Disabilities Act (ADA). As it stands, this stretch of land does not offer an adequate shoulder for even a stroller, bike trailer, wheelchair, or walker, let alone the barrier of protection that the AASHTO- and PROWAG-compliant shared-use side path will provide. We would like to take this opportunity to again mention that a shared-use side path was selected over a separated path in order to meet ADA standards due to the drop-off grade at this location and side slope concerns. In our case, fill dirt can be added to the side of the corridor, all the while staying within MDT's ROW and maintaining a proper slope. On the other hand, designing a separated path due to the grade is quite prohibitive and would limit the inclusive nature of this project.

The *O-Street-to-Bennett Connector Project* includes the following hazards, mitigation measures, and safety benefits.

Hazard	Mitigation	Benefit
Lack of an ADA compliant ramp on O Street	Addition of ADA-compliant ramps with detectable warnings	All pedestrians, including those with certain mobility challenges, will be able to utilize this connection to O St.
No sidewalk along O Street	Addition of an ADA-compliant sidewalk along the <i>East</i> side of O Street, which is perpendicular to Park; this sidewalk will be within the City's ROW and connect to sidewalks on Callender Street, which run parallel to Park (Appendix B)	Pedestrians will be diverted off of the busy section of Park Street, which is lacking sidewalks on the East end, and onto a less trafficked residential area with a sidewalk
Lack of striping/crosswalks and proper signage at O street and Park	An ADA/PROWAG-compliant pedestrian crossing will be added to O Street for future connections	Improved visibility for pedestrians crossing O Street parallel to Park, thus allowing for safer crossing
Driveway Approaches on Highway 89 not meeting slope requirements	Driveway/approaches will be reconstructed to correct for cross-slope requirements	Improved safety for adjacent property owners
Extremely narrow shoulder along Highway 89 South to Veterans Bridge, which was constructed with a multi-use path	By using fill dirt to extend the shoulder, an AASHTO-compliant shared-use path will be engineered for this 0.45 mile portion of the corridor; the shared-use path will include an appropriate 5-foot buffer from traffic as well as a 10-foot path	Individuals with mobility challenges will be provided an alternate means to access the hospital, which is east of Veteran's Bridge and connected to a separated path called the Alpenglow Trail, which joins at the Myer's River View Trailhead and was placed into service in the spring of 2016

As demonstrated in the table above, the *O-Street-to-Bennett Connector Project* will meet ADA requirements and significantly improve usability for all individuals (see [Appendix A – Survey Results](#)). Moreover, our central goal is to develop an ADA-compliant route that is safe for non-drivers, including children, older adults, and individuals with disabilities. All improvements made along this corridor will adhere to accessibility requirements, goals, and guidelines of ADA and MDT, in accordance with state and federal policies, practices, goals, and laws.



CONNECTIVITY

The proposed *O-Street-to-Bennett Connector Project* will provide clear and tangible connectivity benefits to our local transportation environment and offer a safe walking and bike route to an area of critical growth in our community. This connection will also provide a shared-use path to connect Livingston residents, commuters, and visitors to businesses on the east end to Livingston HealthCare – and potential future developments – as well as provide a connection to the trail system (See *Trails Rx Map* [\[Appendix F\]](#) and **Figure 3**).

In September 2013, MDT constructed a new bridge over the Yellowstone River in Livingston that replaced a bridge built in 1934, which was only 22 feet wide and had no provisions for bicycle or pedestrian traffic. The new bridge has a 10-foot wide multi-use path along its south side (**Figure 4**), which has allowed pedestrians and bicyclists to pass over the Yellowstone River for the first time (see **Figure 5** - a photo taken from Veteran's Bridge). However, the stunning bridge overlook is disjointed from the community due to the unsafe stretch on US Highway 89/Park Street. This connection will also provide a safe walking and bike route to the sculptures and wildlife along Myer's River View Trail, which was completed in 2012 and takes individuals directly to the Yellowstone River.

In addition to recent developments (e.g., hospital, bridge, and trail), past planning efforts resulted in the annexation of approximately 1,400 acres of land on the east side of the Yellowstone River (see *Map of Annexations* – [Appendix C](#)). In turn, the proposed path is viewed as vital in promoting both physical and economic developments in what is considered one of our most important planned growth areas; thus, this connection is expected to have a positive influence on commerce (see *Livingston City Planning Board's Letter of Support*, [Appendix H](#), pg. 43). Moreover, this project has been identified by the Active Transportation Coalition as a priority for developing connectivity of existing trails and more thoughtfully addressing City-County interfaces (see *Letter of Support* [Appendix H](#), Pg. 45). Likewise, the proposed project will bring our community one step closer to providing active transportation infrastructure to Livingston Peak and the Absaroka Beartooth Wilderness via Swingley Road (see *Map of Trail Connections East and West* – [Appendix D](#)). While rather short in length, this connection is vital for accessing literally miles of this scenic landscape in southwest Montana through means other than motorized vehicles.



Figure 3: Trail connections to the hospital, Livingston HealthCare.

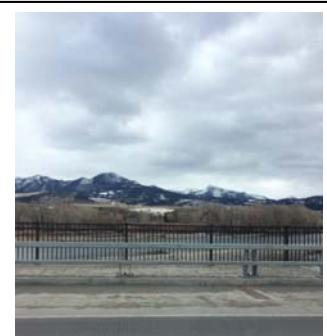


Figure 4: Multi-Use Path on Veteran's Bridge

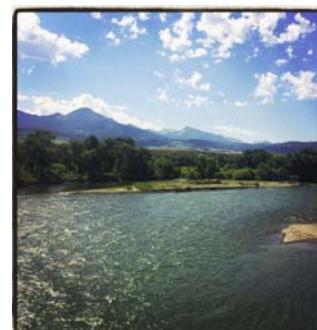


Figure 5: View from Veteran's Bridge



Project Risk Analysis

BUDGET: The budget for this project was developed by our Public Works Director Shannon Holmes, who holds a B.S. in Civil Engineering, Matt Whitman, Public Works Project Manager, and Stahly Engineering, who developed draft plans in 2015 for this project ([Appendix G](#)). The figures provided by Stahly were updated in 2017 by City Engineer Keith Waring to current cost estimates, as well as to include the sidewalk and crossing on O Street and account for an additional 2 feet of shared-use side path to meet AASHTO guidelines. For reference, Stahly's plans account for an eight-foot path with a five-foot buffer, compared to the updated 10-foot path that meets MDT's HSSRA and shared-use path policies. The Engineering Firm TD&H have also reviewed the accuracy of the budget figures for this grant submission. Of note, we have also been in steady contact with MDT personnel, who have kindly assessed the feasibility of these plans and estimates. Moreover, we applied the TA's recommended percentages to calculate the costs for Construction Administration, Preliminary Engineering, and Indirect Costs, with revisions based on the unique nature of this project and reflective of the extensive planning work already performed.

Our Public Works department is well versed in calculating costs associated with sidewalk improvements, as demonstrated by recent related projects, including the 2013 - 2014 TA Sidewalk Project for 5th and E Street and our downtown rebuild to meet ADA. Further, Mr. Holmes' background in engineering and experience working as an engineer for six years for TD&H out of Bozeman, and six more years with AECON Engineering, provide him with the knowledge to evaluate the quote provided by Stahly, update it to current cost estimates, and account for developments such as the addition of a sidewalk and crossing on O Street. Further, the City of Livingston has worked collaboratively on an interdisciplinary level to assure that we have the required 13.42% cash match available, which includes 30% Preliminary Engineering Costs, 20% Construction Administration Costs, 10.98% Indirect Costs, and a 10% Construction Contingency. Please note, we applied a 10% Construction Contingency rather than 20% due to having preliminary design plans for our construction estimate. Our match has the expressed budgetary approval and support of our City Commission and will be financed through our Transportation Impact Fees (*detailed in our 2017 -2021 Capital Improvement Plan; see excerpt in [Appendix K](#)*).

MATCHING FUNDS

	Transportation Alternative Program Fund	City of Livingston Cash Match	Total
Construction	\$152,639.52	\$23,659.30	\$176,298.82
Preliminary Engineering (30%)	\$45,791.86	\$7,097.79	\$52,889.65
Construction Administration (20%)	\$30,527.90	\$4,731.86	\$35,259.76
Right of Way and Utility Costs (0%)	\$0	\$0	\$0
Indirect Costs (10.97%)	\$16,744.56	\$2,595.42	\$19,339.98
Project Total	\$245,703.84	*\$38,084.37	\$283,788.21

A substantial portion of the proposed *O-Street-to-Bennett Connector Project* is within MDT's ROW (80.9% of the project; 0.45 miles); however, we understand that MDT has incurred budget challenges due to stresses on the Highway Special Revenue Account. In order to ensure that this key project will proceed even in the event that State matching funds are unavailable, we are willing to offer a local 13.42% match for the *entire estimated \$283,788 project* (i.e., *\$38,084 cash match). However, should the state receive increased revenue from the Highway Special Revenue Account at the Legislature's discretion, we would ask that MDT cover the portion of the project within its ROW, as is customary for TA, and we would offer a cash match of *\$7,263.73 for our O-Street sidewalk portion of this project. Importantly, on February 22, 2017, our City Commissioners approved the initial *estimated* cash match, reassuring commitment (*see Letter of Support – [Appendix H](#) pgs. 40 – 41*). While this estimated match has increased slightly since this signing, it remains supported by our City governance and within budget allowances outlined in our Transportation Impact Fees ([Appendix K](#)).



PUBLIC INVOLVEMENT

As you will see from the attached Letters of Support ([Appendix H](#)) and our local Survey Results ([Appendix A](#)), this project is firmly backed by our local citizens, the adjacent property and business owners, city and county commissioners, local officials, both the City and County Planning Boards, as well as multiple committees and coalitions. Moreover, this project involves collaboration between two local government entities – the City of Livingston and Park County. Further demonstrating their support, Park County elected not to submit a competing TA application this cycle. More importantly, the public has been well involved and enthusiastic throughout the planning stages of this project and is overwhelmingly in favor of it.

Planning Committees and Task Forces

The *O Street-to-Bennett Connector Project* has been in the works for some time considering that it represents an opportunity to overcome a clear safety hazard and an obvious discontinuity among alternative/multi-modal transportation in Livingston. This discontinuity and the safety issues have become even more evident with the addition of the Myer's River View Trail in 2012, Veteran's Bridge in 2013, and Livingston HealthCare in 2015. Interestingly, even before these developments, and dating as far back as 2006, a task force for the *Livingston/Park County Trails Plan* surveyed residents in zip code 59047 to evaluate community attitudes and desires. According to their results, the greatest demand for trail development involved trail connections. This project also aligns with the more recent *Park County Active Transportation Plan*, *Livingston Trails Rx*, and the *City of Livingston Parks and Trails Master Plan - 2012*. According to the *Livingston Park County Trails Plan (2007 - 2008)*, “[w]orking with MDT during planning for reconstruction of a segment of State Highway 89 and the highway bridge [i.e., Veteran's Bridge] over the Yellowstone River is a primary objective. These two MDT projects have the potential to create a cornerstone of the city/county trails system ... a trails hub that connects trails on all sides of the river and bridge as well as a major arterial route along Park Street.” This project is and has been a long-standing top priority of the Livingston Parks and Trails Committee. Further, it has been discussed monthly at the Livingston/Park County Active Transportation Coalition’s meetings since the coalition’s inception in May 2016. In addition to trail connections, the *O Street-to-Bennett Connector Project* provides a vital alternative transportation means for commuting employees and patients undergoing checkups at Livingston HealthCare and has their full support.

Letters of Support ([Appendix H](#))

Citizens across the community and Park County took the time to author individual letters of support, each telling a unique story. These letters were received promptly, demonstrating a sincere commitment to overcoming this serious safety concern. As with any community, change can be met with doubt and opposition, but that is not the case for this project. We cannot stress enough the astounding support that this proposed project has received, even from the adjacent land and business owners who will be directly impacted during construction. Some individuals even took the time to send hand-written letters and cards to Dave Holien in Helena. Other individuals took time out of their schedules to visit the grant writer’s office in person and further express their support and reiterate the need for this project. As such, the sponsor can stand firmly behind this project, and the community will be grateful to TA for helping it come to being.

Survey Results

Nearly 80% of 522 responses either agreed or strongly agreed with the question “*I would be more likely to walk or ride my bike to the new hospital, Livingston HealthCare, and/or the Myers' River View Trail if a walking and bike path existed between O Street to Bennett (i.e., Rx Coffee to Veteran's Bridge).*” This survey was conducted through mass emailing using a blinded, cross-sectional method, which took place for one month from January to February 2017 ([Appendix A](#)).

Public Communication and Publications

Our local paper, the Livingston Enterprise, reported on the proposed *O-Street-to-Bennett Connector Project* and this grant application in late January 2017 (see pg. 81, [Appendix I](#)). The result was overwhelmingly positive. No negative comments were received by the grant writer. We can firmly assert that we have communicated this project to the public throughout and have been met with strong encouragement.



MDT COORDINATION

The City of Livingston is thankful for its strong and collaborative relationship with MDT. Moreover, we are thankful for having the support of our district, the Butte District, for this particular project, as evidenced in MDT Project Design Engineer Jen Nelson's Letter of Support ([Appendix H](#), pg. 42). As stated clearly by Ms. Nelson, "*[n]o project is without its challenges during production, which is why a strong relationship with the applicant agency is important to project success. The City of Livingston has gone above and beyond in their preparations for this project, taking the initiative to produce conceptual design plans, conducting a user survey and other public outreach in advance of the application, and obtaining letters of support from users and adjacent landowners. They've coordinated with MDT, since late 2015, in a wholly collaborative and mutually-beneficial fashion, and this TA application reflects that coordination.*" We have also worked with MDT to successfully administer past awards (e.g., View Vista Project and 5th and E Street Project) and look forward to the same collaboration for this award.

Knowing the importance that this project has the support of MDT, and the expertise MDT brings to a project, we have included MDT in each key step of our process. Notably, we demonstrated high-level coordination with MDT during their review of the preliminary design plans that Stahly Engineering completed for us 2015 ([Appendix G](#)). We also confirmed that this project will not conflict with any upcoming improvement projects planned by MDT. Most recently, MDT conducted several site visits this past summer to discuss this project, and surveyed and confirmed their ROW ([Appendix J](#)). We also have been in steady contact as needed with Dave Holien since the Summer 2016 regarding our plans to submit a 2017 - 2018 TA application, which has helped further reassure this project's eligibility. Thus, we can firmly assert that we have and will continue to coordinate with MDT for this project, which is in alignment with MDT's transportation goals and policies. Based on our ongoing discussions, we are confident that the proposed project not only aligns with the goals of the TA program, but will significantly increase safety and accessibility within Livingston.

PROJECT INDEPENDENCE

Upon completion, the proposed project will function to its full intent and purpose. In other words, this project is not dependent upon other projects nor will it interfere with other local projects in its vicinity. Further, the *O Street-to-Bennett Connector Project* provides a direct connection to the multi-use path on Veteran's Bridge, a 2013 MDT project. Likewise, the proposed project essentially connects our entire alternative transportation system, or 8.77 miles of City bike and pedestrian trails ([Appendix D](#)). Further, the east end of the trail on Veteran's Bridge connects with the ADA-accessible Myer's River View Trail System, Trails Rx, and the new Alpenglow Trail, which provides an additional, and fully separated path, to Livingston Healthcare (See Trails Rx Map – [Appendix F](#)), thus furthering project independence and connectivity.

The City understands that projects utilizing state or federal grant funding require the sponsor to follow specific guidelines and regulations set forth by the agency in order to help assure project success and sustainability. The City's Administrative Services Director Lisa Lowy and Grant Writer Melody Montgomery have over a decade of combined experience in the development and management of various grant-funded projects. Both are well versed in precisely following guidelines, reporting requirements, and proper documentation processes. Further, the City has effectively managed Federal and State grants including funds from the Federal Emergency Management Agency (FEMA), the Montana Department of Agriculture, the Montana Community Transportation Enhancement Program (CTEP), Safe Routes to School (SRTS), the Montana Department of Natural Resources and Conservation (DNRC), and the Recreational Trails Program (RTP), to name a few. Public Works Program Manager Mr. Matt Whitman will serve as the lead City contact for this project, and will provide prompt and effective project management assistance to MDT should you elect to manage the project or manage firsthand if that be your preference. Ms. Lowy and Ms. Montgomery will provide expert grant management support, and our Public Works Director Shannon Holmes and his team will provide expert technical support.



PROJECT OWNERSHIP AND MAINTENANCE

The City of Livingston Public Works Department will be responsible for the operation and maintenance of the completed project. We have the means and equipment to successfully maintain this critical connector and will implement a signed Maintenance Agreement in coordination with MDT. Our maintenance activities will be funded through our Street Maintenance Budget. We will comply with the stipulations outlined in Policy 8-03-001 in Section 4.4 (Shared-Use Path Policy). We would like to express our willingness to accept ownership and long-term maintenance responsibilities for connector project.

We have developed expert guidance to accept ownership of this project through our *Trail Asset Management Plan*, which was completed in 2015. Our Public Works Department had this plan developed in order to help address future costs related to trail maintenance and development in the City of Livingston, and factor these costs into future Capital Improvement Plans (CIPs). According to this Trail Asset Management Plan, currently Livingston spends approximately \$13,680 per year on routine maintenance activities for existing trails, which is equivalent to approximately \$1,710 per mile of trail. We can apply this figure to help determine a reasonable annual budget for maintenance activities.

Our asset management plan also recommends that a customized maintenance plan be developed for each new trail, which will also be the case for this project. We will do so using a template that MDT has already provided, and modify it to meet the needs of this project and its standard trail characteristics (e.g., surface type, width, location and length). This plan will also budget for long-term costs associated with this project. We also recognize that trails have life cycles, and we will work to extend this to its full capacity by resurfacing as agreed upon in the Maintenance Agreement and/or overlay in addition to regular crack seals as needed. These activities will be built into our CIP and Street Maintenance Fund. We will also complete routine tasks such as snow removal and sweeping, as we do for our other assets.

PROJECT RIGHT-OF-WAY (ROW)

The ROWs for this project have been secured and are free of conflicts. The proposed project involves the following two key elements: a sidewalk along the East side of O Street to Callender, for which the City owns the ROW, and a shared-use path along 0.45 miles of US Highway 89 South, which is entirely within MDT's ROW. Further, as noted in the ADA section, given MDT's rather wide 100-foot ROW, fill dirt can be added to the corridor while staying within MDT's ROW and maintaining a proper slope. Thus, MDT's ROW provides ample surface area for the construction of the proposed shared-use path and will resolve encroachment issues. Moreover, in order to delineate MDT's ROW, Kevin Christensen, MDT's District Surveyor, marked the area of interest in May 2016 using GPS, which can be viewed in the album of MDT's photos in [Appendix J](#).

Even though the majority of this project falls within MDT's ROW, we understand the importance of having support from adjacent land and business owners. For this reason, we have openly discussed the project with these affected individuals, who have enthusiastically offered their support and encouragement. Moreover, they are aware of the encroachment issues that will be resolved through this project (*please see Letters of Support, Appendix H – Chris Benden, Property Owner, pg. 66, and Diane Coffman, Business Owner at Rx Coffee, pg. 65*). Fortunately, there are only two approaches/driveways that receive limited travel and will have a very limited impact on pedestrians or bicyclists; however, they will need to be resurfaced to meet the correct grade. Despite these construction inconveniences, affected land owners are agreeable and more importantly, know that this project represents a vital safety connection for our community. As such, we can again assert to you that the ROWs are secure and free of conflicts.

PROJECT UTILITY IMPACTS

Stahly Engineering, the firm who developed the preliminary design plans, first considered utility impacts and completed a utility locate as part of their design process. The related utilities are marked in pages C0.2, C1.1, C2.1 of the design plans ([Appendix G](#)). We have also forwarded these preliminary design plans onto Northwestern Energy. Internally, we have taken additional steps to determine if existing utilities will be negatively impacted by the proposed project. Overall, our Public Works Department determined, in collaboration with Stahly Engineering, that while there are utilities present, they will not need to be relocated



due to the nature of the project and therefore, are not expected to be negatively impacted. As an additional step, we reached out to MDT's District Utility Engineering Specialists Janet Black and Joe Walsh who helped confirm these initial assessments. Again, while utilities are present, as demonstrated in the photos below, this project will be designed so that utilities are not negatively impacted. Utilities are located primarily near the O Street and Bennett intersection, where the proposed crosswalk will be added. While the final design has yet to be completed, it is reasonable to expect that concrete can be poured around the existing fire hydrant and light pole for the sidewalk and crossing. With respect to the light pole, overhead utility line, and fire hydrant, 5 - 10 feet clearance can be left on one side (or "bulb out"), which will be in compliance with the United States Proposed Guidelines for Pedestrian Facilities in the Public Right-of-Way (PROWAG). Further, fiber optic cables will not be impacted considering that the project involves fill dirt rather than excavation. Again, these details will be resolved in the final design to prevent negative utility impacts. Lastly, should an unexpected utility conflict arise, we will take all necessary measures to ensure that the necessary mitigation occurs and that the project moves forward to completion.

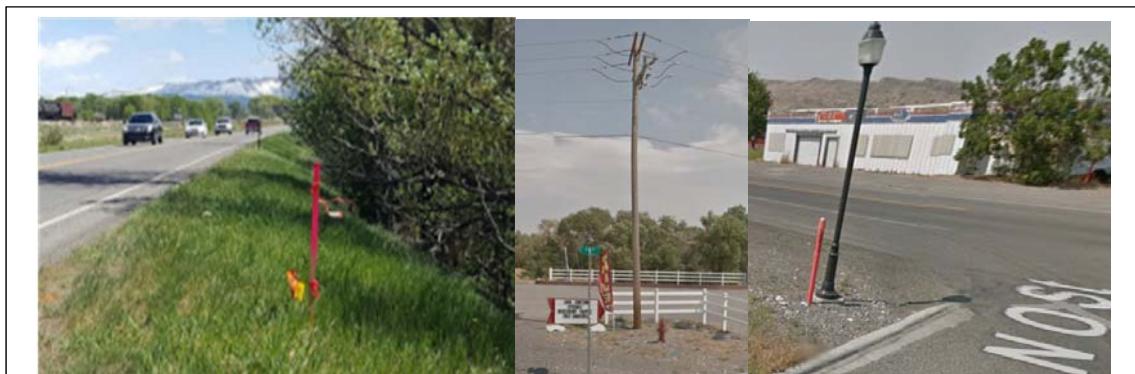


Figure 6: Utilities

PROJECT SUMMARY/IMPACT

Overall, this project represents a critical multi-modal safety connection for Livingston residents to access the new hospital, Livingston Healthcare, which opened its doors in October 2015. This connection will also provide a safe walking and bike route to the existing path on Veteran's Bridge, Myer's River View Trail, Alpenglow Trail, and distant connections in Park County, including wilderness areas. This project has further implications for potential future developments with respect to land annexed east of the hospital (*see Appendix C*). Importantly, we have the support of MDT, our local citizens, city and county commissioners, and multiple committees and coalitions. As you can see from these letters, this connection represents a key opportunity to overcome a known safety issue on US Highway 89 South/Park Street. We hope that you will consider our proposal favorably, and we sincerely thank you for your time to review this proposal.

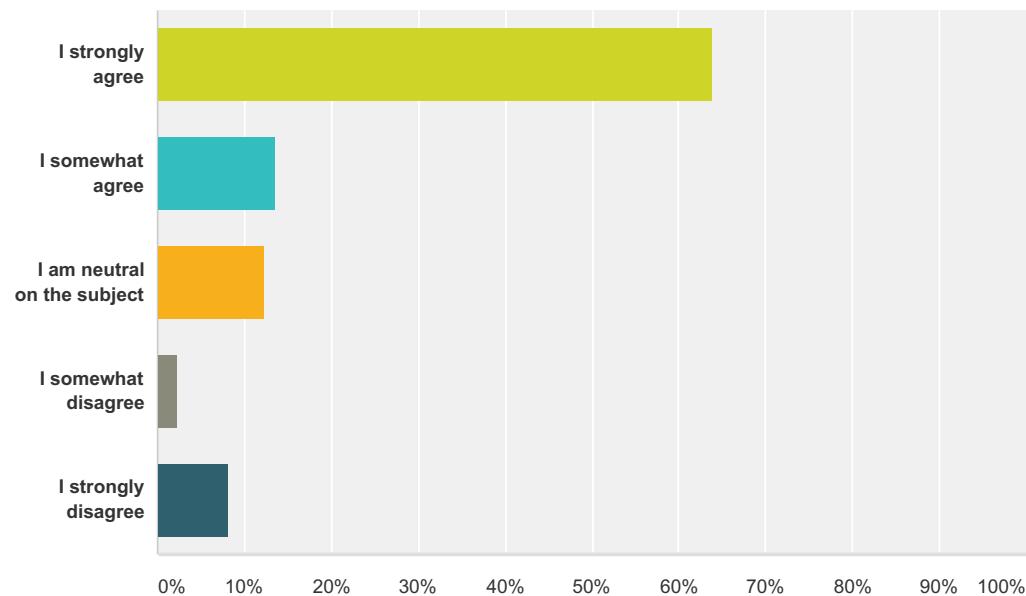
Appendix A

Survey Results

O Street to Bennett Connector

Q1 I would be more likely to walk or ride my bike to the new hospital, Livingston HealthCare, and/or the Myers' River View Trail if a walking and bike path existed between O Street to Bennett (i.e., Rx Coffee to Veteran's Bridge).

Answered: 522 Skipped: 0



Answer Choices	Responses
I strongly agree	63.79%
I somewhat agree	13.60%
I am neutral on the subject	12.26%
I somewhat disagree	2.30%
I strongly disagree	8.05%
Total	522

Appendix B

Map of Area of Interest

O Street to Bennett



Legend

— O_street

— Narrow Shoulder with Steep Embankment

Proposed O Street Trail

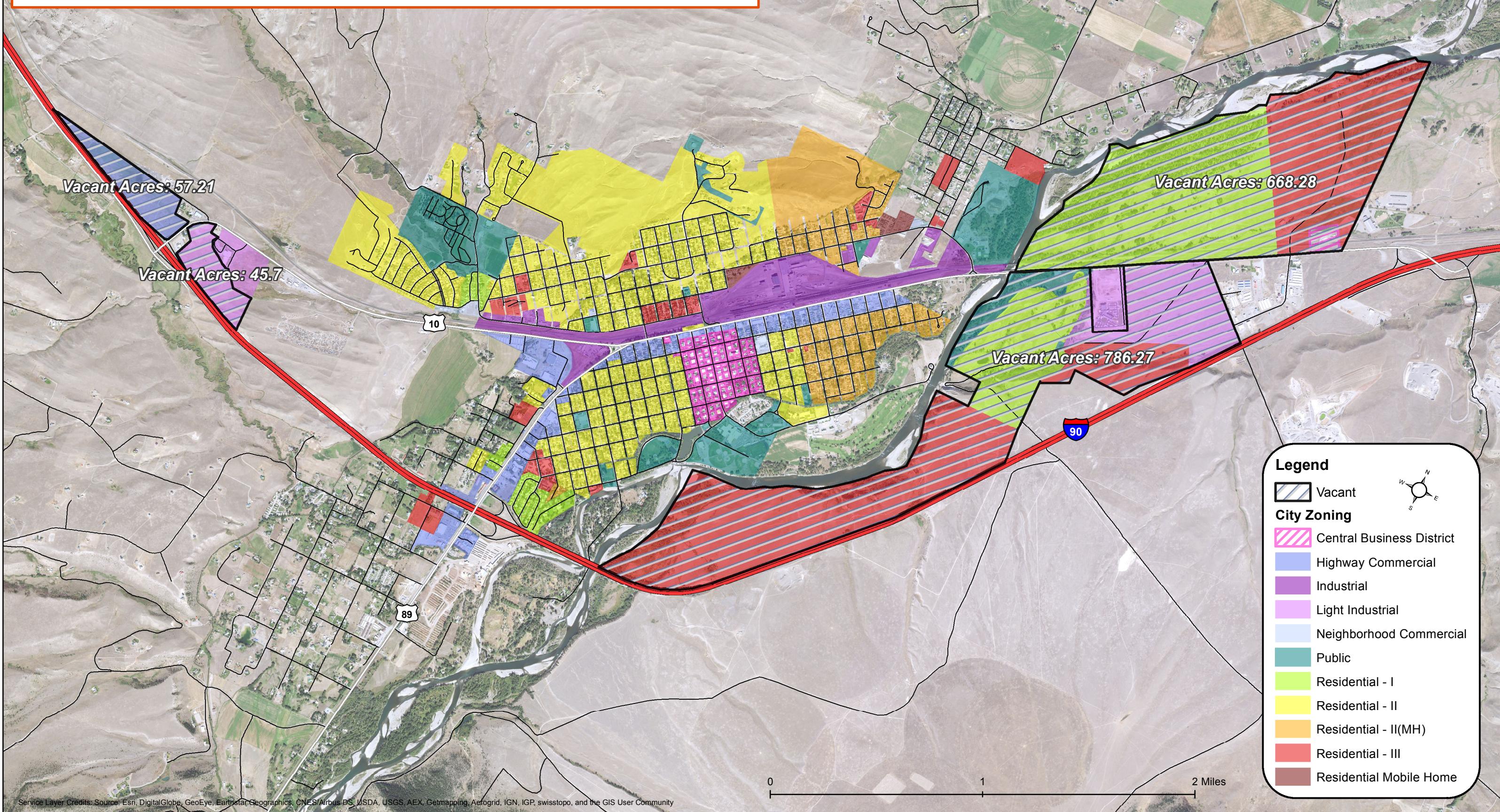
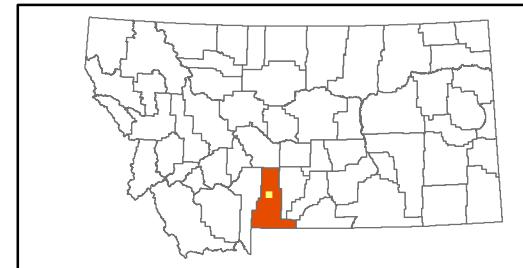
0 0.025 0.05 0.1 0.15 0.2 Miles

Appendix C

Map of Annexations and Growth Areas

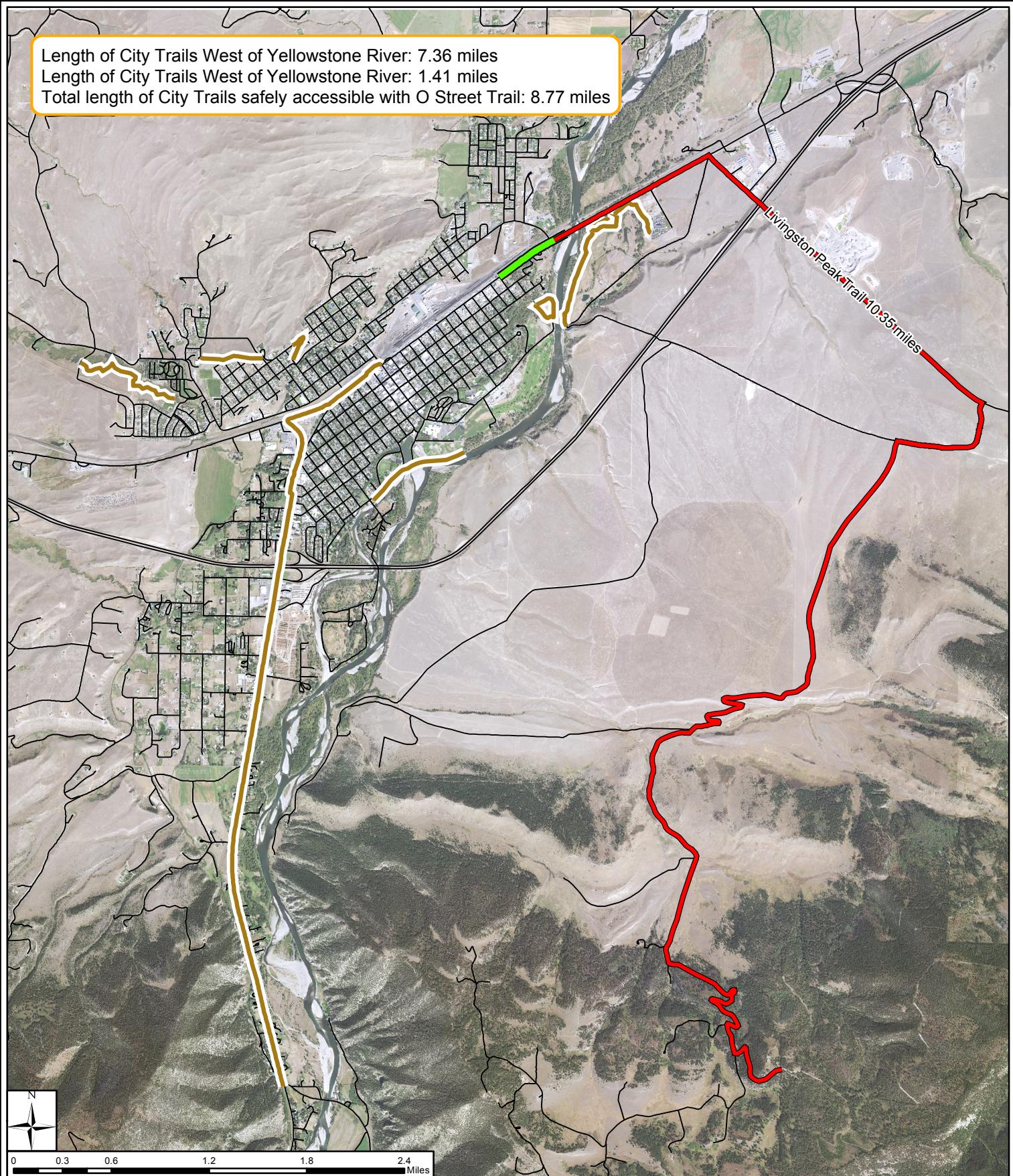


Vacant Annexations Livingston Montana 2017



Appendix D

Map of Tail Connections East and West



Proposed O Street Trail

Connect trails on the West Side
of the Yellowstone River to trails
on the East Side

- Livingston Peak Trail
- O Street Trail
- Official City Trails

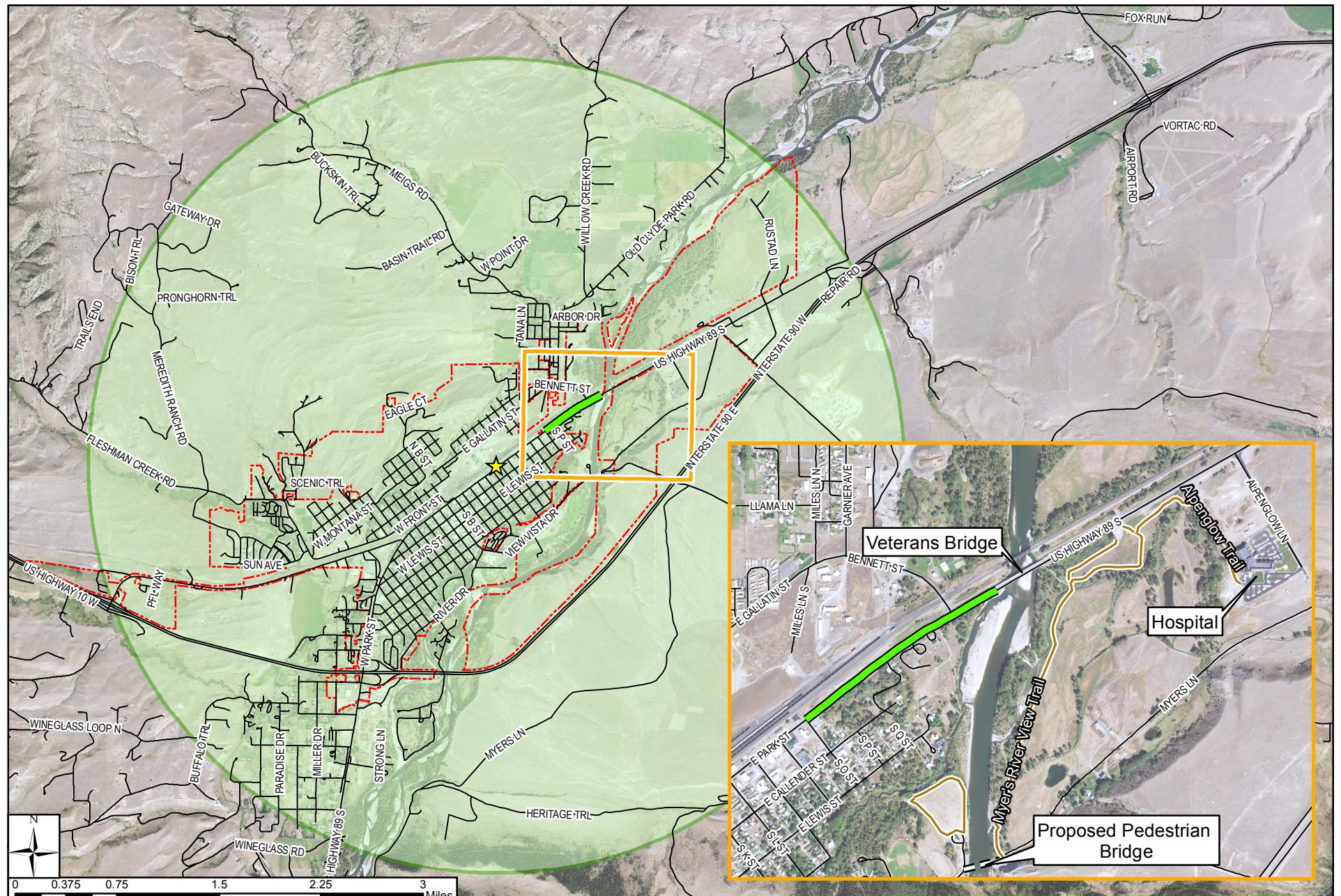
LIVINGSTON
Montana

GO BEYOND YELLOWSTONE

This map is for informational purposes only and not for legal, engineering or surveying purposes. The City of Livingston assumes no legal responsibility for this information and shall not be liable for any claims or damages arising out of the use of this information.
Date: 2/13/2017
Author: SJAY

Appendix E

Map of Geographical Center of Livingston



Proposed O Street Trail

A safe connection to access
trails east of the Yellowstone River

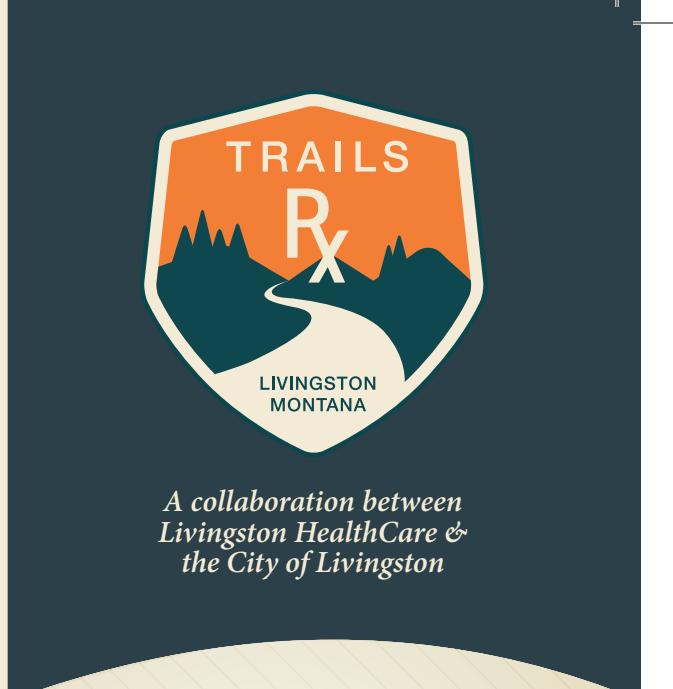
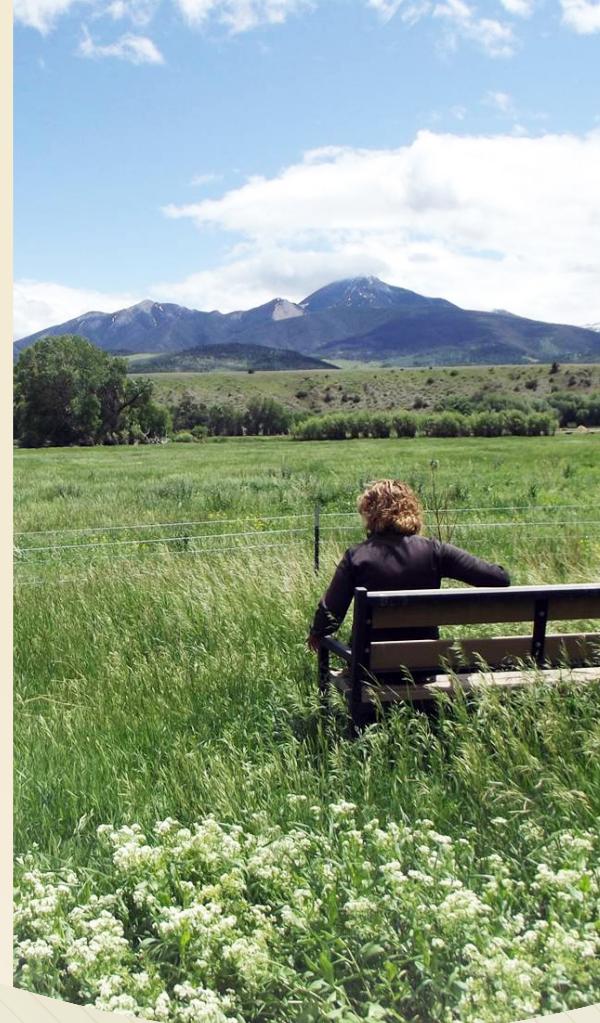
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Date: 2/13/2017
Author: SJAY

Appendix F

Trails Rx Map



1 Bozeman Trail Connector 1 MILE

The Bozeman Trail is a gravel walking path funded by Montana Fish, Wildlife, & Parks (FWP) Recreational Trails Program. A scenic connecting route from Livingston's Northern Lights Subdivision to Fleshman Creek Road, the Bozeman Trail is one of Livingston's most popular destinations for walkers, bikers, and runners. This trail contains several notable features including a lake and marsh habitat at the half mile mark, as well as another small lake at the trail's end.

2 Bitterroot Trail ½ MILE

Bitterroot is a gravel walking path, funded by Montana FWP Recreational Trails Program with trees and landscaping generously donated by Northwestern Energy. The Bitterroot Trail is part of a network designed to connect the Northside Soccer Complex to adjoining residential neighborhoods, providing a convenient and safe route to and from one of Livingston's most frequently used recreational spaces.

3 Summit Trail ½ MILE

Summit Trail is a gravel walking path funded by Montana FWP Recreational Trails Program. Located only ¼ mile from Summit Street, the trail was built similarly to Bitterroot Trail and designed to connect the adjoining parks, trails, and residential neighborhoods. This trail provides moderate hiking and 360° views of Absaroka, Crazy, and Bridger Mountain ranges.

4 Front Street Trail 1 MILE

The Front Street Trail provides a safe alternative for connecting pedestrians from the south side of Livingston to the Washington School, Northside Park and Soccer Fields, Legion Field and other recreational opportunities in the northwest part of Livingston. This trail was funded by the Montana FWP Recreational Trails Program. The trail starts at the intersection of 5th and Front Street and continues along the south side of Front Street to Star Road. This trail project also included the 0.4-mile Northside Trail Connector which connects the Northside Park and Soccer Fields to the Bozeman Trail. It traverses along North Lights Road and provides access to Mars Park and the Legion Field.

5 Myers' River View Trail 1 MILE

The Myers' River View Trail is a gravel walking path funded by Montana FWP Recreational Trails Program and completed in 2012. A donation from the Kodak Foundation purchased detailed nature signs featuring native wildlife photographs and information. The Artemis Institute constructed a trail head and hidden trail features that blend in with the natural surroundings. Some of the features include windpipe whistles, natural benches, and a lookout over the river. Myers' River View Trail borders the Yellowstone River and, when connected with the Alpenglow Trail, provides almost 3 miles of trail. With stunning views of the Yellowstone, Myers' River View Trail is a perfect destination for a scenic stroll, a family picnic or an afternoon run.

6 Alpenglow Trail ½ MILE

The newly formed Alpenglow Trail is a gravel walking path, funded by Montana FWP Recreational Trails Program. This trail connects Livingston HealthCare (LHC) to Myers' River View Trail. The trail was a collaborative effort between the City of Livingston and LHC, as it is the first city trail to be partially located on private property. The trail features a pedestrian bridge that was generously constructed in-kind by Clarke Smith, in memory of his late wife and LHC employee, Lori A. Smith.

7 Yellowstone River Levy ¾ MILE

The Yellowstone River Levy is a paved and gravel walking path constructed by the City of Livingston with the help and direction of the Army of Corps of Engineers. The levy borders the Yellowstone, providing river access and offering breathtaking views. This trail is centrally located, close to Sacajawea Park, the Band Shell, and Miles Park. The nearby lagoon offers bird and wildlife viewing for all ages!

8 Bike Path 4½ MILE

The Highway 89 South Bike Path is a paved trail funded by the City of Livingston. Starting near downtown Livingston on North 5th Street—and following Park Street until it becomes Highway 89 South and intersects with East River Road at the head of Paradise Valley—this path is the best option for a longer run or bike ride, with plenty of beautiful and varied scenery.

A recreational trail guide for Livingston, Montana

Trails Rx is a collaborative program between the City of Livingston and Livingston HealthCare designed to promote outdoor activity and trail use in Livingston.

The Livingston Trails Rx program intends to unite natural landscape and healthy living by educating our community of the wellness benefits of trail recreation.

Walk, run, hike, bike, and stroll through Livingston's trail system and enjoy our beautiful town, as well as a healthy body and mind!

Any proceeds generated from the Livingston Trails Rx Program will be used to create new trails.



For more Livingston Trails Rx information and event listings, **visit www.LivingstonTrailsRx.com**. Livingston parks and trails are designated tobacco-free zones. Please do not use tobacco in these areas. Trails Rx maps were made possible through the Montana Fish, Wildlife, and Parks Recreational Trail Program.

Appendix G

Preliminary Design Plans

Stahly 2015

PLANS FOR CONSTRUCTION OF:

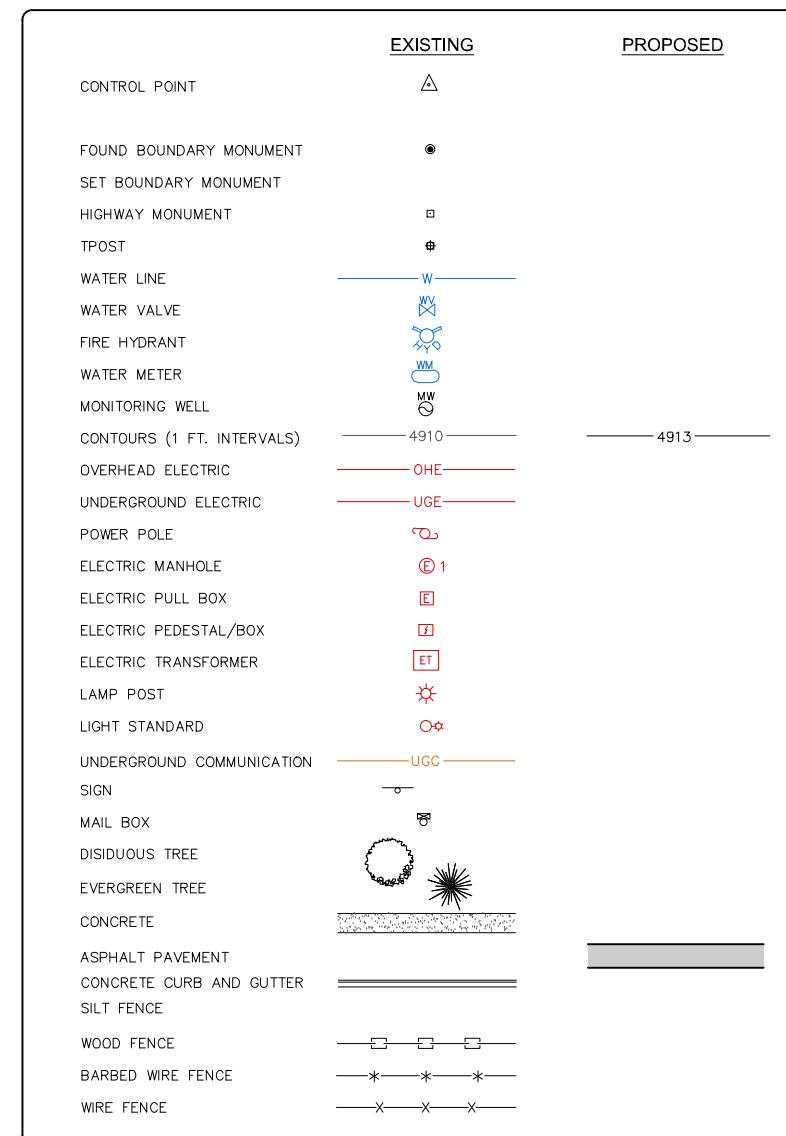
O STREET TRAIL

CONSTRUCTION PLANS

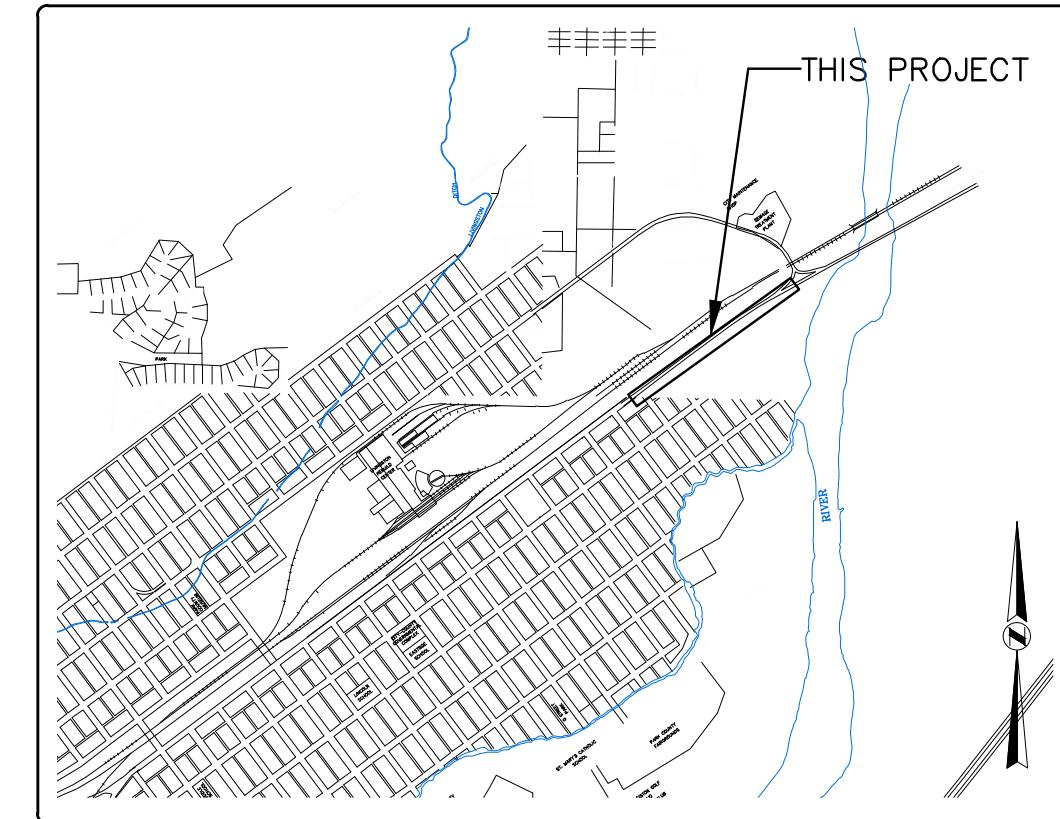
LIVINGSTON, MONTANA

SHEET INDEX	
Sheet Number	Sheet Title
T1.0	TITLE SHEET
C0.1	CIVIL SPECIFICATIONS & DETAILS
C0.2	OVERALL SITE PLAN
C1.1	EXISTING CONDITIONS & DEMOLITION PLAN
C1.2	EXISTING CONDITIONS & DEMOLITION
C2.1	PROPOSED TRAIL
C2.2	PROPOSED TRAIL

LEGEND



PROJECT LOCATION



ABBREVIATIONS

C	CENTERLINE	PVC	POLYVINYL CHLORIDE
CO	CLEANOUT	PVI	POINT OF VERTICAL INTERSECTION
DIA	DIAMETER	R & R	REMOVE AND REPLACE
EX	EXISTING	S	SANITARY
FL	FLOW LINE	SCH	SCHEDULE
FT	FEET	SD	STORM DRAIN
HDPE	HIGH-DENSITY POLYETHYLENE	SDI	STORM DRAIN INLET
HP	HIGH POINT	SDMH	STORM DRAIN MANHOLE
LP	LOW POINT	STA	STATION
MAX	MAXIMUM	TBC	TOP BACK OF CURB
MEG	MATCH EXIST GRADE	TBW	TOP BACK OF WALK
MH	MANHOLE	TFW	TOP FACE OF WALK
MIN	MINIMUM	TYP	TYPICAL
PT	POINT OF TANGENT	VC	VERTICAL CURVE
		W	WATER

PROJECT CONTACTS

ENGINEER
Stahly Engineering & Associates Kurt Thomson, PE. - Project Engineer 522-9526 Office 599-0775 Mobile
CITY OF LIVINGSTON
Public Works and Utilities Shannon Holmes, Public Works Director 222-5667 Office
DESIGNED: KWT DRAWN: APD CHECKED: --- DATE: 6-7-2015



STAHLY
ENGINEERING
& ASSOCIATES

PROFESSIONAL
ENGINEERS &
SURVEYORS
www.seeng.com

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HELENA, MT 59601
Phone: (406) 442-8594
Fax: (406) 442-8557

851 BRIDGER DR. STE. 1
BOZEMAN, MT 59715 Phone: (406) 522-9526
Fax: (406) 522-9528

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NOT FOR CONSTRUCTION

TITLE SHEET

O STREET TRAIL
LIVINGSTON, MT

CITY OF LIVINGSTON

DESIGNED: KWT
DRAWN: APD
CHECKED: ---
DATE: 6-7-2015
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CIVIL
SPECIFICATIONS
& DETAILS

O STREET TRAIL
LIVINGSTON, MT

CITY OF LIVINGSTON

DESIGNED: KWT
DRAWN: APD
CHECKED: ----
DATE: 6-7-2015

SHEET

C0.1

MATERIALS SUBMITTALS REQUIRED

PRODUCT	PARTY RESPONSIBLE FOR SUBMITTAL	REQUIRED SUBMITTALS PRIOR TO MOBILIZATION	REQUIRED SUBMITTALS DURING CONSTRUCTION
ON SITE ROAD SUBGRADE MATERIAL	CONTRACTOR	NONE	1 REPRESENTATIVE PROCTOR FOR EACH TYPE OF MATERIAL ENCOUNTERED
ROAD SUB-BASE	CONTRACTOR	GRADATIONS, PROCTOR, LIQUID/PLASTIC LIMITS, P.I.	SEE MATERIALS TESTING
CRUSHED BASE COURSE	CONTRACTOR	GRADATIONS, PROCTOR, LIQUID/PLASTIC LIMITS, P.I., FRACTURED FACES, WEAR.	SEE MATERIALS TESTING
ASPHALT	CONTRACTOR	PROJECT MIX DESIGN FROM SUPPLIER	SEE MATERIALS TESTING

ON-SITE MATERIALS TESTING

PRODUCT	PARTY RESPONSIBLE TO OBTAIN TESTS	TEST FREQUENCY	TEST REQUIREMENT & STANDARD
ROAD/PARKING SUBGRADE	ENGINEER	1 TEST PER 100 LINEAR FEET OR 10,000 SQUARE FEET	95% BY AASHTO T99 OR ASTM D698 PER MPWSS
SUB-BASE COURSE	ENGINEER	1 TEST PER 100 LINEAR FEET OR 10,000 SQUARE FEET	95% BY AASHTO T99 PER MPWSS
CRUSHED BASE COURSE	ENGINEER	1 TEST PER 100 LINEAR FEET OR 10,000 SQUARE FEET	95% BY AASHTO T99 PER MPWSS
ASPHALT MIX VERIFICATION	ENGINEER	1 TEST PER 50 LINEAR FEET OR 5,000 SQUARE FEET	93% BY ASTM D2041 PER MPWSS

CIVIL NOTES

1) CIVIL SITE IMPROVEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH MONTANA PUBLIC WORKS STANDARD SPECIFICATIONS (MPWSS), 6th EDITION AND THE CITY OF LIVINGSTON STANDARD MODIFICATIONS.

2) THE LOCATION OF EXISTING WATER LINES, SEWERS, TANKS, CULVERTS AND OTHER UTILITIES AS SHOWN ARE APPROXIMATE. REASONABLE ASSUMPTIONS HAVE BEEN MADE REGARDING THE LOCATION OF THESE FACILITIES. MINOR CHANGES IN LOCATIONS WILL NOT CONSTITUTE A CHANGE OF CONDITIONS. CONTRACTOR SHALL VERIFY THE LOCATION AND DEPTH OF THESE FACILITIES AS NECESSARY TO MAKE CONNECTIONS TO OR TO EXCAVATE ADJACENT TO THESE FACILITIES. THE ENGINEER MAY REQUIRE THE CONTRACTOR TO EXPOSE EXISTING WATER AND SEWER LINES TO ESTABLISH THE EXACT LOCATION OF THESE UTILITIES.

3) UNDERGROUND UTILITY LOCATIONS ARE APPROXIMATE AND ARE NOT TO BE CONSIDERED COMPLETE. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL BURIED UTILITIES PRIOR TO BEGINNING CONSTRUCTION. THE CONTRACTOR SHALL PROTECT ALL EXISTING UTILITY LINES DURING CONSTRUCTION. ALL UTILITY LINES DAMAGED DURING CONSTRUCTION ACTIVITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. OTHER UNDERGROUND UTILITIES MAY EXIST, THE CONTRACTOR SHALL HAVE ALL UTILITIES MARKED BY THE UTILITY COMPANY AND/OR ONE-CALL LOCATORS. CALL BEFORE YOU DIG.

1-800-424-5555

4) NOTIFY ENGINEER OF ANY DISCREPANCIES WITH THE PLANS AND THE ENGINEER WILL PROVIDE A REPRESENTATIVE FOR FIELD VERIFICATION. THE ENGINEER CAN BE REACHED AT:

STAHLY ENGINEERING & ASSOCIATES INC.
851 BRIDGER DRIVE, SUITE 1
BOZEMAN, MT. 59715
PHONE (406) 522-9526
FAX (406) 522-9528

5) ALL CONSTRUCTION SHALL BE CONDUCTED FROM WITHIN EXISTING STREET EASEMENTS, UNLESS CONTRACTOR HAS RECEIVED WRITTEN PERMISSION FROM ADJACENT PROPERTY OWNERS.

6) CONTRACTOR SHALL PROVIDE A TRAFFIC CONTROL PLAN TO THE ENGINEER PRIOR TO CONSTRUCTION.

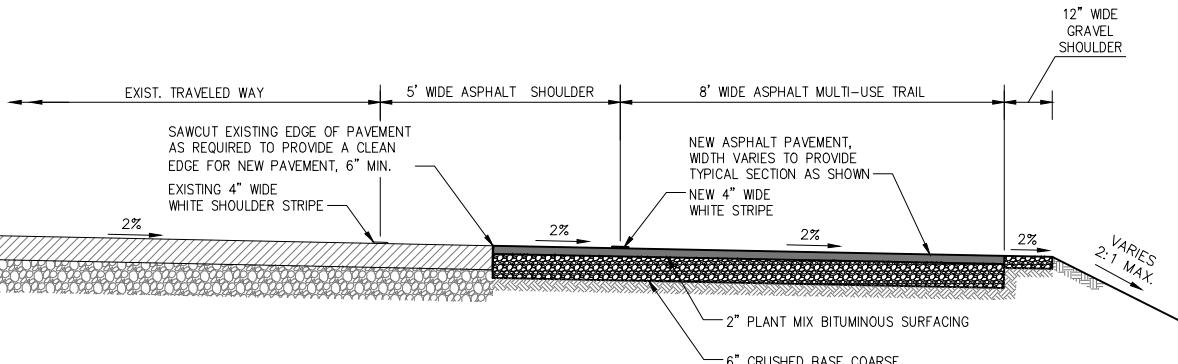
7) CONTRACTOR SHALL TAKE PRECAUTIONS TO PREVENT DAMAGE TO EXISTING PAVEMENT. ANY DAMAGE TO EXISTING PAVEMENT OUTSIDE OF PROJECT LIMITS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. SAWCUT ALL EXISTING PAVEMENT EDGES AND TACK PRIOR TO PLACING NEW PAVEMENT.

11) ALL EXISTING TREES TO REMAIN UNLESS DIRECTED BY THE CITY OF LIVINGSTON OR THE ENGINEER TO BE REMOVED. EXISTING TREES TO BE PROTECTED AT ALL TIMES DURING CONSTRUCTION.

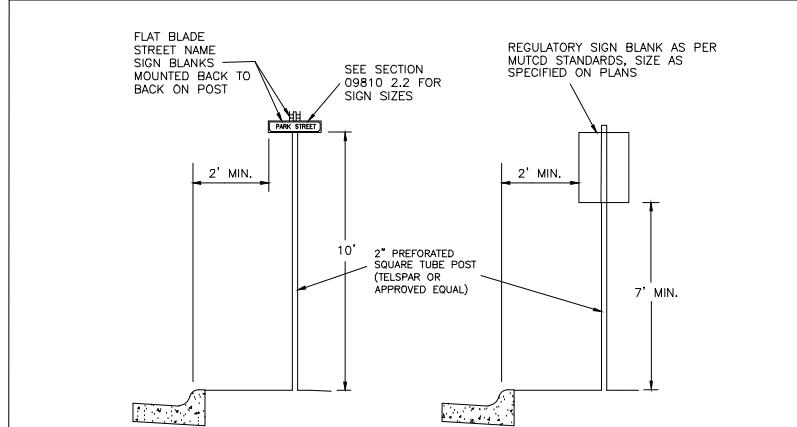
12) CONTRACTOR TO INSTALL STREET SIGN BASE AND POSTS ONLY. CITY OF LIVINGSTON TO INSTALL PLACARDS.

13) ALL CONCRETE & ASPHALT PAVEMENT REMOVAL/DEMOLITION SHALL BE IN ACCORDANCE WITH MPWSS SECTION 02112 UNLESS OTHERWISE NOTED.

14) ALL CONCRETE CONSTRUCTION SHALL BE IN ACCORDANCE WITH MPWSS SECTION 02528 AND 02529 UNLESS OTHERWISE NOTED.



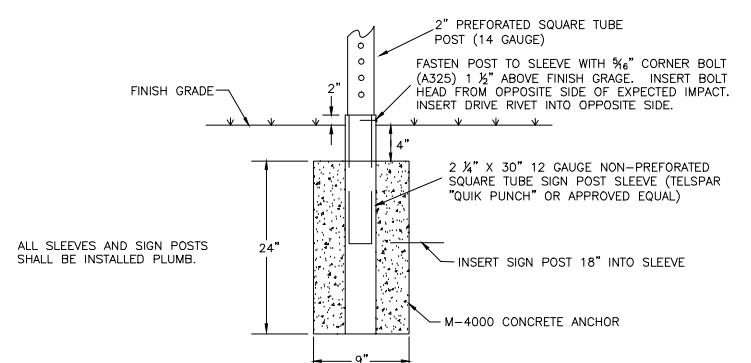
TYPICAL SECTION



ATTACH SIGNS TO POST WITH $\frac{1}{8}$ " DRIVE RIVETS (MIN. 2 PER SIGN)

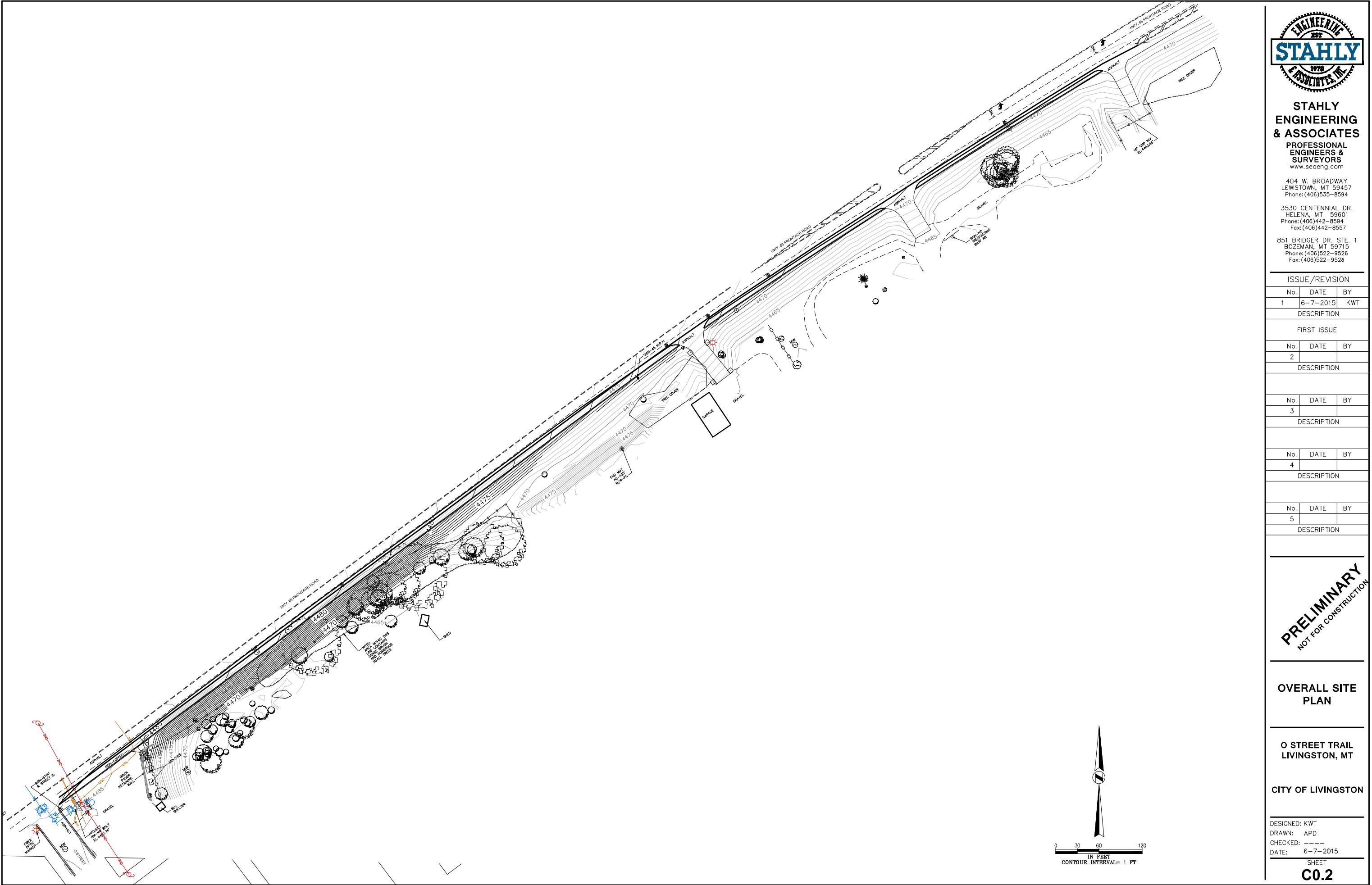
STREET MARKER SIGN

REGULATORY SIGN



SIGN POST FOUNDATION DETAIL

CITY OF LIVINGSTON STANDARD DRAWING	SCALE: NONE	SIGN INSTALLATION STANDARDS	NO. 09810-1 APRIL 2014





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Phone: (406) 442-8594
Fax: (406) 442-8557

851 BRIDGER DR. STE. 1
BOZEMAN, MT 59715
Phone: (406) 522-9526
Fax: (406) 522-9528

ISSUE/REVISION
No. DATE BY
1 6-7-2015 KWT

DESCRIPTION

FIRST ISSUE
No. DATE BY
2

DESCRIPTION

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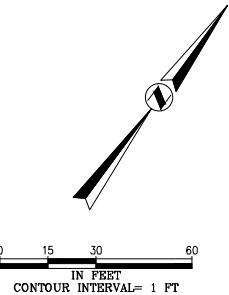
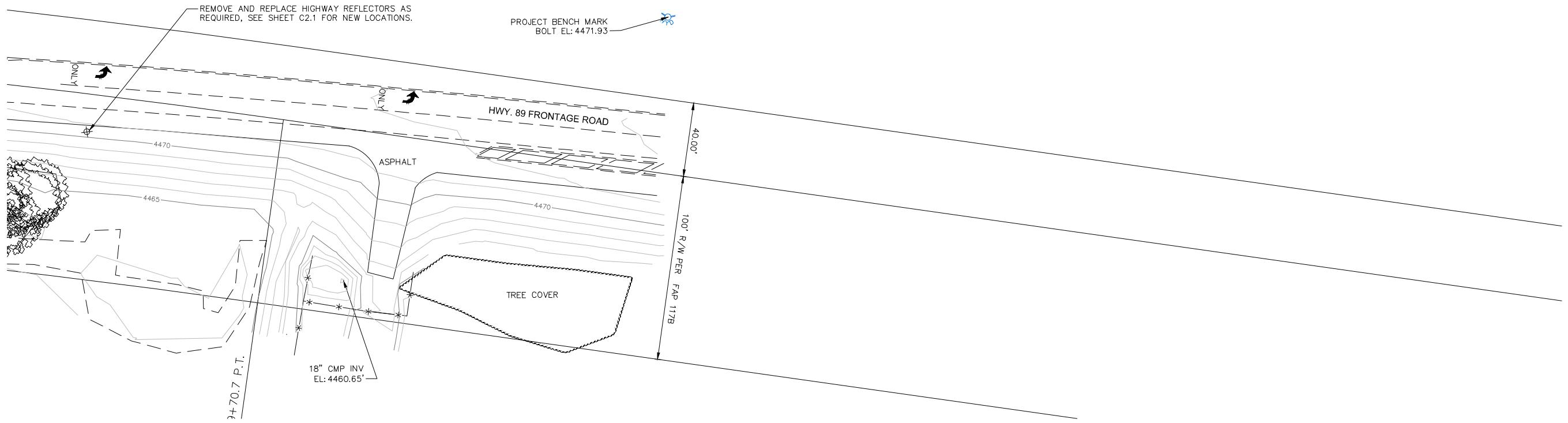
O STREET TRAIL
LIVINGSTON, MT

CITY OF LIVINGSTON

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DRAWN: DRAWN
CHECKED: CHECKED
DATE: 6-7-2015

SHEET

C1.2



C1.2



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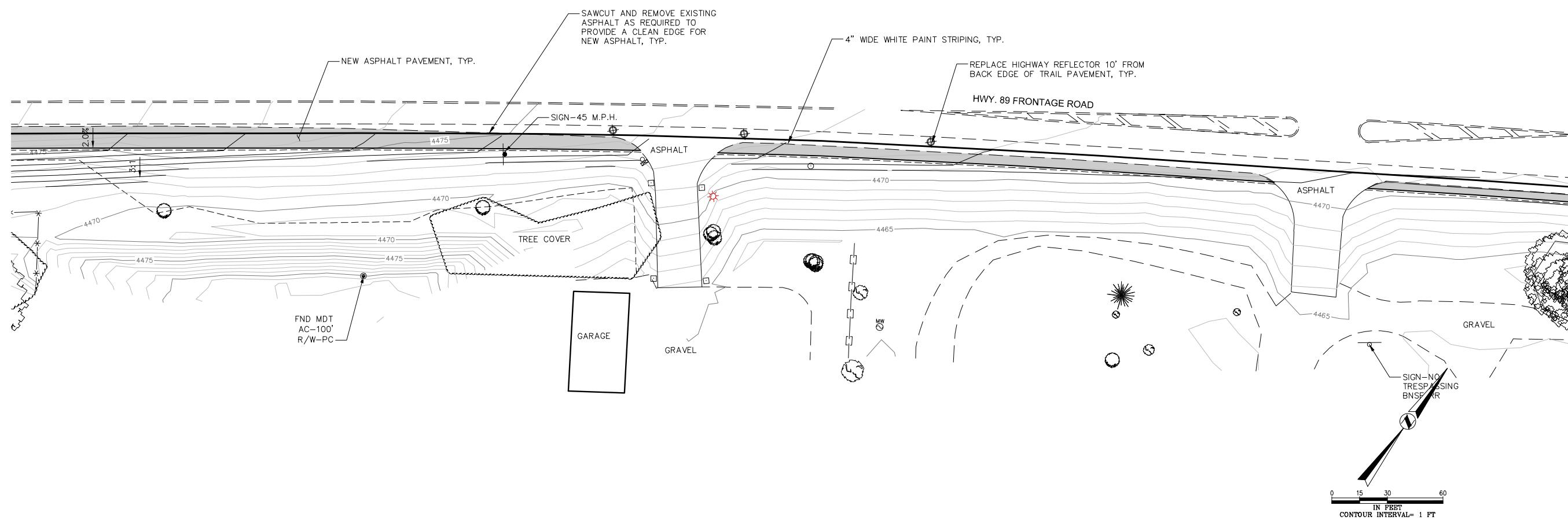
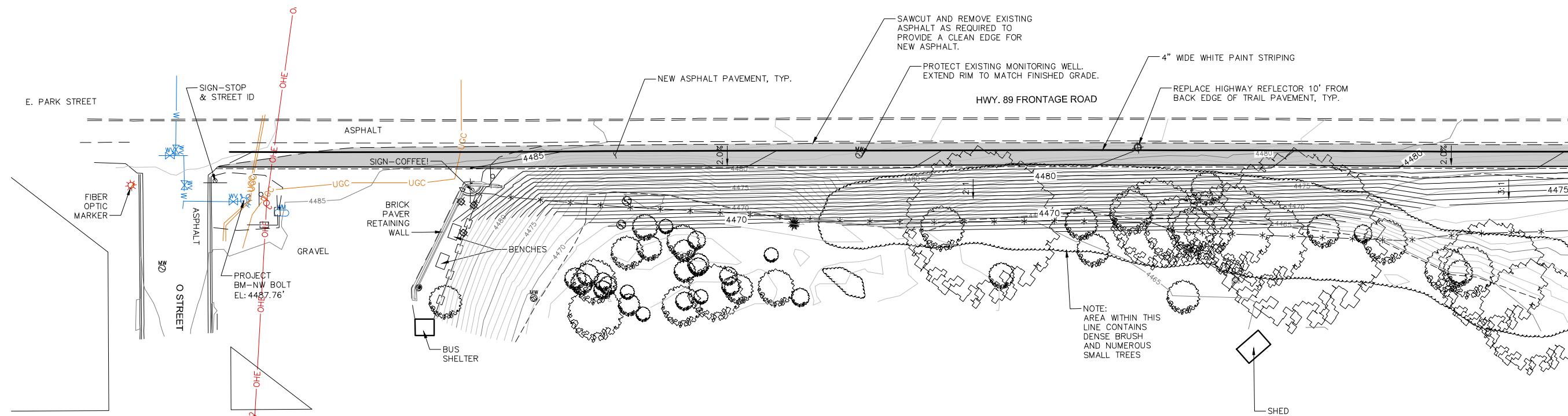
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LIVINGSTON, MT

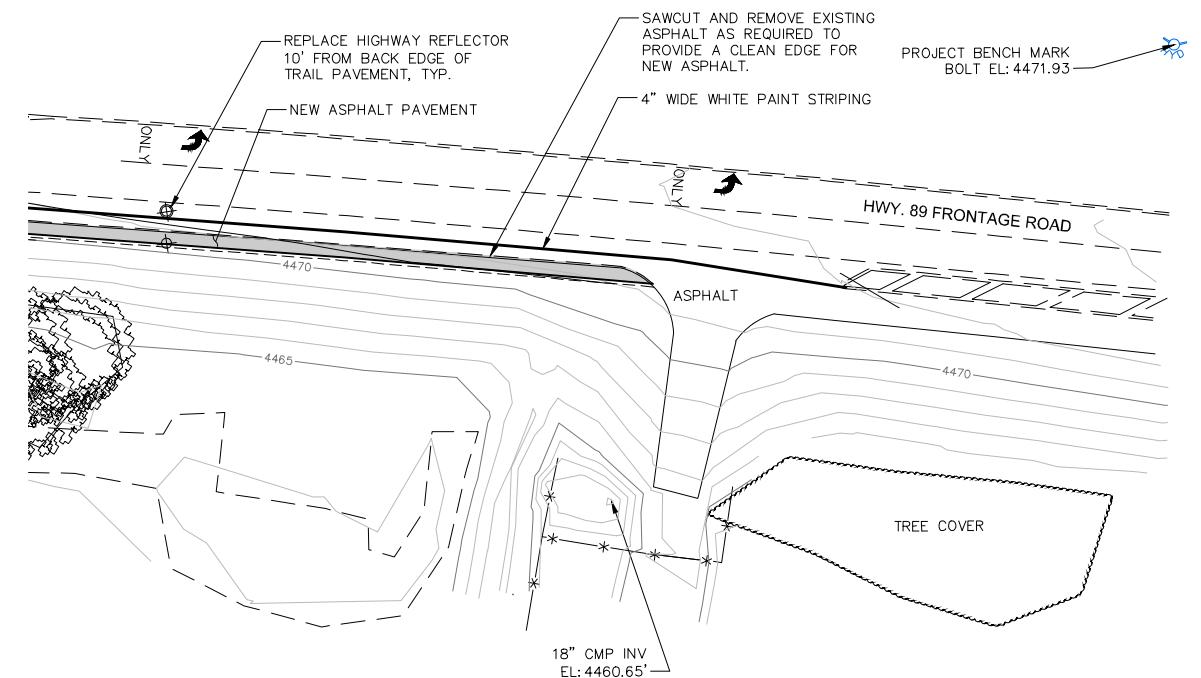
CITY OF LIVINGSTON

DESIGNED: KWT
DRAWN: APD
CHECKED: _____
DATE: 6-7-2015

SHEET

C2.1





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DESCRIPTION

PRELIMINARY
NOT FOR CONSTRUCTION

PROPOSED TRAIL

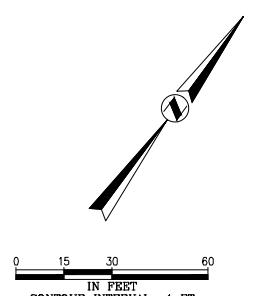
O STREET TRAIL
LIVINGSTON, MT

CITY OF LIVINGSTON

DESIGNED: KWT
DRAWN: APD
CHECKED: ---
DATE: 6-7-2015

SHEET

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Appendix H

Letters of Support

Letters of Support Index

O Street-to-Bennett Multi-Use Path and Connector Project

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City Manager
Lisa Lowy

414 East Callender Street
Livingston, Montana 59047
(406) 222-2005 phone
(406) 222-6823 fax
citymanager@livingstonmontana.org
www.livingstonmontana.org



Chairman
James Bennett

Vice Chairman
Dorel Hoglund

Commissioners
Mel Friedman
Quentin Schwarz
Sarah Sandberg

Incorporated 1889

February 22, 2017

Mr. Dave Holien
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena, MT 59620-1001

Dear Review Committee,

The City of Livingston is writing to express our strong support and commitment to the proposed *O Street-to-Bennett Multi-Use Path and Connector Project*. This project represents an integral connection for our community. As you can see from the letters of support and survey results, we have support from local citizens, city and county commissioners and officials and multiple committees and coalitions. This project is a critical multi-modal safety connection for Livingston residents to access the new hospital, Livingston Healthcare, which opened its doors in October 2015.

This connection will also provide a safe walking and bike route to the stunning vista from Veteran's Bridge, as well as the sculptures and wildlife along Meyer's River View Trail, which takes individuals directly to the Yellowstone River. However, due to limits in our budgets, we are unable to fund it in its entirety; we also understand that, because it is an MDT asset, this project must be funded through the State even if we were to procure private funding. Thus, to prevent a tragedy in our community and enhance our alternative transportation routes, we ask for your favorable review.

Approximately $\frac{1}{2}$ mile of the proposed project is in MDT's ROW; however, we understand that MDT has incurred budget challenges in the Highway Special Revenue Account. In order to ensure that this key project will not be cancelled in the event that State matching funds are unavailable, we are willing to offer a local 13.42% match for the entire estimated \$235,907 project (i.e., \$31,659 cash match), which will be funded through our Transportation Impact Fees. However, should the State's budget challenges be resolved, we would ask that MDT remit funds for this portion of the project in its ROW, as per standard protocol, and we would provide a cash match of \$6,769 for the O Street sidewalk portion, which is not within MDT's ROW. Additionally, we have the means and equipment to successfully maintain this critical connector and will implement a signed

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Montana
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Maintenance Agreement, as discussed in the proposal. We would also like to note that, throughout this process, we have been in steady communication with MDT to proactively overcome this known safety issue. Our partners at MDT are aware of this proposed project and offer their full support.

In summary, the purpose of our local government is to maintain and improve safety, quality of life, a sustainable economy, and the public's interest and trust. Our mission is to ensure a high quality of life by keeping our community well maintained, well informed, and working together. The proposed project aligns with both our purpose and our vision. We hope that you will consider our proposal favorably, and we thank you for your time. Please do not hesitate to contact our Interim City Manager Lisa Lowy at 406-823-9870 or llowy@livingstonmontana.org should you have any questions regarding our commitment to this project.

Sincerely,

A handwritten signature in blue ink that reads "James Bennett". The signature is fluid and cursive, with a prominent "J" at the beginning.

James Bennett, Chairman
Livingston City Commission

Dave,

I am writing on behalf of the Butte District to express support for Livingston's TA project application for the "O-Street-to-Bennett Connector Project". This project will fill in a missing connection in Livingston's bicycle and pedestrian transportation network and enhance safety for users of all modes. The District believes that the proposed sidepath is the best solution to address the connectivity, safety, and access issues given exiting roadway and site configuration. The proposed facility meets criteria outlined in recent MDT policies (HSSRA and Shared-Use Path), including serving a transportation purpose (as evidenced by existing usage and user survey), being within a city or town, and enhancing transportation safety. The project additionally presents the District with opportunities to resolve existing encroachments with the benefit of local agency support and to improve the facility's side-slopes and, hence, overall maintainability.

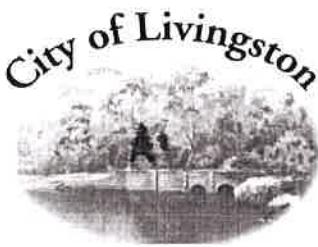
No project is without its challenges during production, which is why a strong relationship with the applicant agency is important to project success. The City of Livingston has gone above and beyond in their preparations for this project, taking the initiative to produce conceptual design plans, conducting a user survey and other public outreach in advance of the application, and obtaining letters of support from users and adjacent landowners. They've coordinated with MDT, since late 2015, in a wholly collaborative and mutually-beneficial fashion, and this TA application reflects that coordination.

We appreciate the opportunity to provide input about the proposed TA projects in our District and look forward to the opportunity to see this project to fruition.

Warm Regards,

Jen

Interim City Manager
Lisa L. Lowy
citymanager@livingstonmontana.org
(406) 823-6000 Phone



Chairman
James Bennett
Vice Chairman
Dorel Hoglund
Commissioners
Mel Friedman
Sarah Sandberg
Quentin Schwarz

February 17, 2017

Mr. Dave Holien
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena, MT 59620-1001

Dear Review Committee,

The Livingston City Planning Board is writing to express our strong support for the proposed *O Street-to- Bennett Multi-Use Path and Connector Project*. This project represents an integral connection for our community. As you can see from the application and survey results this project has wide-ranging support from the local community, city and county commissioners and officials and multiple committees and coalitions. This project represents a critical multi-modal safety connection for Livingston residents to access the new hospital, Livingston Healthcare, which opened its doors in October 2015.

This connection will also provide a safe walking and bike route to a critical growth area for our community. Our past planning efforts have led to the annexation of approximately 1,400 acres of land on the east side of the Yellowstone River and this path is seen as vital in promoting both physical and economic development in one of our most important, planned growth areas. This project was identified several years ago by the City's Parks and Trails Committee as a top funding priority and the opportunity to bring this connection to fruition furthers the goals of our Parks and Trails Masterplan and the City Growth Policy.

In summary, the Livingston City Planning Board is in full support of this project. Multi-modal transportation systems, pedestrian facilities and bicycle paths are an integral part of all of the Planning Board's land use decisions and recommendations. The proposed project aligns with our existing planning documents and our future vision. We hope that you will consider this proposal favorably, and we thank you for your time. Please do not hesitate to contact our Director of Building & Planning at 406-222-4903 or jwoodhull@livingstonmontana.org should you have any questions regarding our support for this project.

Sincerely,

Jim Woodhull
Planning Director

LIVINGSTON
Montana

GO BEYOND YELLOWSTONE

414 East Callender Street
Livingston, MT 59047
www.livingstonmontana.org



Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena MT, 59620-1001

January 31, 2017

Dear Mr. Holien,

The Park County Commission wishes to express full support for the City of Livingston's Grant Application to the Montana Transportation Alternatives Program to assist with funding for the proposed *O Street-to-Bennett Multi-Use Path and Connector Project*.

The proposal to construct a sidewalk with a pedestrian crossing on O Street will improve public safety by diverting pedestrian and bike traffic off of US Highway 89 South/Park Street and onto Callender Street via O Street. The proposed connection will provide a bike and walking lane to connect Livingston businesses and residents to Livingston HealthCare as well as providing a connection between the trail system east of Veteran's Bridge and trails surrounding Meyers Landing and along the Yellowstone River. The project brings our community one step closer to providing active transportation infrastructure to Livingston Peak and the Absaroka Beartooth Wilderness via Swingley Road.

This proposed trail extension supports our Park County Active Transportation Plan strategic priorities, including:

- Healthy and Safe Alternative Transportation Promotion
- Parks, Trails and Recreation Network Opportunities
- Effective Collaboration and Management
- Positive Economic Competitiveness

We would greatly appreciate your support to increase healthy outdoor activity, public health and safety and further community development in Livingston and Park County.

Thank you for your consideration of this project and for your coordination of this program.

Best,


Steve Caldwell
Chairman


Clint Tinsley
Commissioner


Bill Berg
Commissioner

Park County-City of Livingston Active Transportation Coalition
c/o Jacqueline Isaly
Health Promotion & Outreach
Park County Health Department
414 E Callender
Livingston, MT 59047

January 27, 2017

Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena, MT 59620-1001

Dear Mr. Holien,

Park County-City of Livingston Active Transportation Coalition would like to take this opportunity express our full support for the City's Grant Application to the Montana Transportation Alternatives Program to fund the proposed *O Street-to-Bennett Multi-Use Path and Connector Project*. This project has been identified by the Active Transportation Coalition as a priority for developing connectivity of existing trails and more thoughtfully addressing City-County interfaces. We are enthusiastic about the potential of this projects as it aligns well with the mission of our coalition.

The Active Transportation Coalition is a collective and collaborative group of key stakeholders from all sectors with the charge to help guide policy for a well thought out, well designed, accessible, and active community. Members include City of Livingston and Park County, the MSU Extension Office, business owners, community members, and local organizations. The goals of the coalition focus primarily on connectivity, planning, and collaboration.

This project represents an integral component of our mission and will provide a critical safety element. We hope that you will consider the strong merit of this proposed connection and that you will consider full funding of this request. If you would like any more information regarding my support of this project, please contact me at 406-222-4113.

Sincerely,



Jacqueline Isaly
Chair, Park County-City of Livingston Active Transportation Coalition



Park County Planning Department
414 East Callender St.
Livingston, MT 59047

February 7, 2017

c/o Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena MT, 59620-1001

Dear Mr. Holien:

As Planning Director of the Park County Planning Department it is my pleasure to send a letter in support of the City of Livingston's Grant Application to the Montana Transportation Alternatives Program for funding assistance to further connectivity and safety in our vibrant, growing community.

This project has been long sought by our active community. It will not only enhance recreational opportunities through increasing connectivity for an existing trail system, it will also provide an alternative means for individuals to access the new hospital and has further implications for other development prospects on the east side of town. This is particularly important as it has been identified as an area of concern by our joint City/County Active Transportation Committee, our Parks Board, and is in line with the Park County Active Transportation Plan.

It is encouraging to see the progression in expansion activities of alternative transportation means in our rural community. I hope that you will consider this proposal favorably. Please contact me at 406-222-4102 should you have any questions.

Sincerely,

A handwritten signature in blue ink, appearing to read "William Michael Inman".

William Michael Inman, CFM
Planning Director



BUSINESS
IMPROVEMENT
DISTRICT

2015-2016 BOARD OF TRUSTEES

January 19, 2017

David Holien
Acting TA Program Manager
Montana Department of Transportation
PO Box 201001
Helena, MT 59620-1001

KRISTEN GALBRAITH
CHAIR

DALE SEXTON
VICE-CHAIR

BETTY LAHREN

DAN KAUL

DAVID DEPUY

KARLA PETTIT

TYLER ERICKSON

Dear Mr. Holein,

As Chair of the Livingston Business Improvement District, I am writing to express support for the City of Livingston's application for the Montana Transportation Alternatives Program. It is my understanding that these funds will assist with funding a project to broaden the shoulder on the narrow section of Park Street, from where Rx coffee and the Jefferson Line bus stop to Veteran's Bridge on US Highway 89 South. I also understand the project will also include a pedestrian crossing on O Street and nearby sidewalk area to help divert pedestrian and bike traffic off of the busy US Highway 89 and onto Callender Street. Overall, the proposed connection will provide a bike and walking lane to connect Livingston businesses on the east end of town to Livingston HealthCare - and potential future developments - as well as provide a connection to the trail system just east of Veteran's Bridge.

The Livingston Business Improvement District was formed in early 2011 with goals to perform a variety of services to improve the image of Livingston and promote the individual and unique businesses located in the downtown area. The District also seeks to carry out economic development services by working to attract, retain and expand businesses. It is encouraging for the District to see the development and implementation of projects that focus on connectivity of trails which encourage increased recreation and active transportation. The project will provide a positive influence to commerce and development adjacent to and within the District. Construction of the trail area, with added safety features, will ultimately contribute to the overall success of our revitalization and economic development efforts.

Again, this project is fully supported by the District and we thank you for considering the City of Livingston's application request. Please contact me at 406-220-9600 should you have any questions regarding our support.

Yours,

A handwritten signature in black ink that reads "Kristen Galbraith".

Kristen Galbraith
Chair



January 19, 2017

Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
P.O. Box 201001
Helena, MT 59620-1001

Dear Mr. Holien,

As the owner of a retail business in Livingston that specializes in hiking, biking, skiing and climbing, and as a community member that actively supports non-motorized transportation, we adamantly support the City of Livingston Grant Application to the Montana Transportation Alternatives Program.

We understand this Grant would be instrumental in providing funds for broadening the shoulder on the narrow section of US Hwy 89 S/Park Street, and also provide a pedestrian crossing on O Street and sidewalk allowing access to Callender Street. Safety and use will be greatly enhanced with these improvements.

As a lifelong Livingston resident, avid cyclist, and advocate for non-motorized transportation, the City proposal would greatly increase safety and use for pedestrians and cyclists along this corridor. Of all transportation corridors within our area, this one in particular currently is one of the most utilized and least safe, and thus, in great need of improvement. Thank you for considering our letter of support and the grant application.

Sincerely,

Dale Sexton



Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena MT, 59620-1001

January 23, 2017

Dear Mr. Holien,

We, Livingston Trails Rx, are writing to express our full support for the City of Livingston's Grant Application to the Montana Transportation Alternatives Program to assist with funding for the proposed *O Street-to-Bennett Multi-Use Path and Connector Project*. This project provides a key connection for our **Trails Rx Program**, which is a collaborative effort between the City of Livingston and Livingston HealthCare (**LHC**). Our program is designed to promote healthy outdoor activity and trail use in Livingston. Our *mission* is to encourage physical activity through the Livingston parks and trails system. The proposed project will provide a missing link to connect our many local trails as we strive to promote overall health in our community.

The section of *US Highway 89* that Livingston proposes to widen and make accessible for alternative transportation means will be invaluable to our program. In the Spring of 2016, we completed the ribbon cutting for a bridge that connects LHC's trail to City of Livingston's Meyers River Trail. The next time you visit Livingston, we highly recommend that you visit this trail system. It is laced with sculptures and educational material, and takes visitors on a peaceful walk down to the Yellowstone River, where they can be immersed in native wildlife and flora. However, without the proposed connection, this trail is disjointed from town, making it safest to access by driving a motorized vehicle to the trailhead or hospital. The *O Street-to-Bennett Multi-Use Path and Connector Project* would allow individuals to safely access these, and other trails (Please see map <http://www.livingstontrailsrx.com/trail-guide.html>). Moreover, the additional bike path and sidewalk along O Street would further promote health and alternative modes transportation.

In summary, the Livingston Trails Rx program works to unite natural landscape and healthy living by educating our community of the wellness benefits of trail recreation. This project provides an integral link to our mission and is a critical safety element. We hope that you will consider the strong merit of this proposed connection and score this project favorably.

Sincerely,

Trails Rx

Maggie Tarr,

Director of the Livingston Recreation Department



PO Box 1766
Livingston, MT 59047
www.nightowlrun.com

January 26, 2017

Mr. Dave Holien
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena, MT 59620-1001

RE: Montana Transportation Alternatives Program

Dear Mr. Holien:

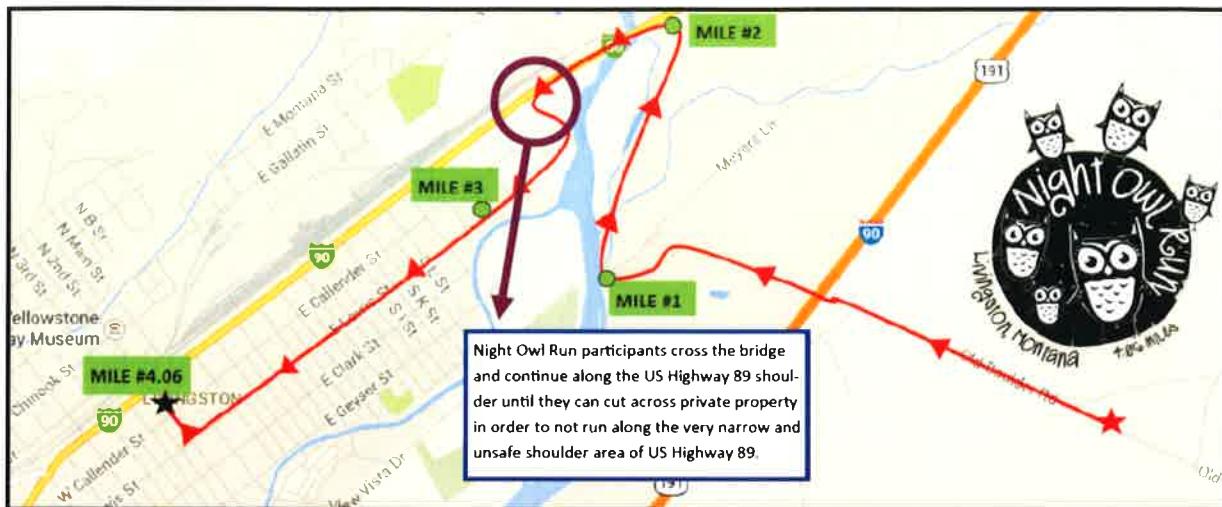
I am happy to extend my support for the City of Livingston O Street-to-Bennett Multi-Use Path and Connector Project application. I know the proposed project will provide safe and adequate transportation alternatives to pedestrian and bicycle traffic.

The Night Owl Run is a community event in its eighth year of raising funds for various Park County groups and programs. Over the past seven years, the event has raised over \$38,000 – all of which has been donated back to various youth, education and recreation related programs and organizations within Park County. In addition to community fundraising goals, the Night Owl Run group is very interested in expanding and connecting existing trails throughout Park County which will ultimately encourage more walkers, runners and bikers to utilize a growing network of trail systems. It is exciting to know that the City of Livingston is pursuing a project that will encourage healthy and safe transportation alternatives.

The Night Owl Run course is a worthwhile trek, beginning on the flats shadowed by the Absaroka Mountains, traveling under Interstate 90, traversing along the banks of the Yellowstone River, crossing the US Highway 89 Veteran's Bridge and then continuing west into town along a 4.06-mile course. The major safety issue we have with this event each year is the area that this City of Livingston project application focuses on. We always request assistance from local volunteers (minimum of 3 people with safety vests and flags) and of Park County Rural Fire District #1 (with a truck) to be present at the junction of the area west of the bridge and the old KPRK station in order to divert runners onto private property. We feel this is the safest route to get those runners and walkers (365 participants in 2016) off a highway with heavy traffic and little to no shoulder (6" in width in some areas)! Adding a pedestrian lane would not only be beneficial to the many users of the east end trail network, it also allows our event to consider an alternate route should use of the private property not be feasible.



PO Box 1766
Livingston, MT 59047
www.nightowlrun.com



The proposed project meets the goals and objectives of the Montana Transportation Alternatives Program and I hope you will consider their application favorably. Please feel free to contact me if you have additional questions regarding my support of a very exciting trail extension project.

Yours,

Kristen Galbraith

Race Director

info@nightowlrun.com



January 31, 2017

Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena MT, 59620-1001

**Park County
Extension Office**

Mr. Holien:

I am writing on behalf of Montana State University (MSU) Park County Extension to support the City of Livingston's Grant Application to the Montana Transportation Alternatives Program for funding to assist with connecting our community and improving safety along a particularly dangerous portion of our main transportation corridor. It is my pleasure to support this application to further connectivity and livability in our vibrant and scenic community.

Safety along a particularly dangerous corridor is a critical component of this proposal, yet it also have positive economic impacts in Livingston. Quality of life is an important factor in supporting economic development activities. With the construction of the new, award-winning Livingston HealthCare facility on the east end of Livingston, this area is poised for new development and growth. Proactive multi-use transportation planning and development will ensure Livingston is prepared to meet the needs of this growth.

The City of Livingston's Grant Application to the Montana Transportation Alternatives Program has my full support. I hope that you consider this application favorably. Please contact me at 406-599-4822 or katie.weaver@montana.edu if you have any questions regarding my support.

Sincerely,

A handwritten signature in black ink, appearing to read 'Katie Weaver'.

Katie Weaver
Economic and Community Development Agent

119 South 3rd Street
Livingston, MT 59047
www.msuetension.org

Tel (406) 222-4156
Fax (406) 222-4199
E-mail park@montana.edu
parkcountyextension.org



Bike Walk Montana, Inc.

PO Box 584
Helena, MT 59624
406-449-2787

Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena, MT 59620

February 1, 2017

Dear Dave,

Bike Walk Montana is a statewide non-profit organization dedicated to making bicycling and walking safe and accessible for all. We strive to build relationships with state agencies and communities to help everyone get a better understanding of the important role non-motorized transportation plays in Montana. In order for non-motorized transportation to continue thriving in our communities it is a necessity that safe connections, paths, crosswalks, shoulders, and sidewalks are included in current infrastructure. As an advocate for safe active transportation our organization greatly supports the City of Livingston's Grant Application to the Montana Transportation Alternatives Program. Funding assistance for the O Street-to-Bennett Multi-Use Path and Connector Project will improve non-motorized livability in Livingston's scenic, active, and growing community.

Bike Walk Montana has many active members that reside in Livingston. Many of the members rely solely on active transportation as a means of getting to and from their jobs, stores, and other daily activities. All of the members whom we have spoken with in regards to this project have expressed sincere concern for their safety when traveling on the narrow section of US Hwy. 89 S. Many have stated that the fear of being on a bicycle on this stretch of road is interfering with their love for commuting in their community. The residents of Livingston wish to continue their active healthy lifestyles, but are afraid to do so without the improvements being sought after by the City of Livingston. Given the current lack of connectivity to the hospital, it is only a matter of time before someone is hit while walking or bicycling; please consider fully funding this project before it does.

It says great things about Montana and the residents of our rural communities that we desire active lifestyles. I sincerely hope you will consider full funding of this request as the City of Livingston has clearly met the requirements in the MDT application guidelines. A safe way to actively engage in this vibrant community will greatly improve the health and lives of the residents and visitors of Livingston. Please contact me at (406) 449-2787 if you have any questions regarding Bike Walk Montana's full support of this well planned project.

Sincerely,

Melinda Barnes
Executive Director



Bike Walk Montana – making bicycling and walking safe and accessible for everyone.
Become a member at www.bikewalkmontana.org



Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
Helena MT, 59620-1001

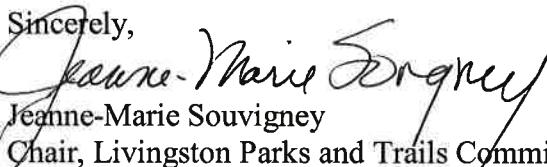
In September 2013, Livingston acquired a new, modern bridge that replaced one built in 1934. The old bridge was only 22-feet wide and had no provisions for pedestrian or bicycle traffic. Now, there are twelve-foot driving lanes and eight-foot wide shoulders one each side. There is also a 10-foot wide multi-use path along the south side of the bridge. Pedestrians and bicyclists can now cross over the Yellowstone River, something that was not possible in the past. However, there is one major obstacle to a safe walking/biking route to the east side of the river. The proposed *O Street-to-Bennett Multi-Use Path and Connector* will overcome this obstacle.

When Park Street/US Highway 89 South, the main arterial road through Livingston, was updated and widened, the section of road between O Street and the new bridge was left its original, narrow width. Along this approximately half-mile section, there is no shoulder to speak of and steep drop-offs. At some points, the slope is approximately 45 degrees with a drop of about 20 feet. It is important to note that even with these drop offs, this road is heavily used and serves as the only bypass for traffic on I-90. Weather and accidents often cause closure of the interstate and traffic is detoured through here. The narrow road presents a danger to vehicular traffic and is especially hazardous to pedestrians and bicyclists.

There are several establishments on the east side of the river that would benefit from remediating this safety hazard. Situated here is a new \$43.5 million hospital that encompasses 125,000 square feet. It serves 70,000 to 80,000 patients a year and is Livingston's major employer. Many of its employees have expressed interest in having a safe and healthy alternative route to get to work. Further up the road, Stafford Animal Shelter employs approximately 20 and has a steady stream of visitors looking to adopt pets or volunteer. There is also a rapidly growing industrial park. Thus, the east interchange area is a great opportunity for other commercial enterprises.

A recent addition to Livingston's Parks and Trails, Myers' River View Trail parallels the east bank of the Yellowstone River. Amenities include a mile-long gravel path with scenic views of the river. Natural benches, wind pipe whistles, and whimsical structures designed and built by students from MSU's School of Architecture are enjoyed by numerous residents and visitors alike. Livingston Healthcare, Trails RX, and Livingston Public Works have partnered to build trails and a beautifully designed footbridge to connect the hospital to the park.

The City of Livingston is committed to seeing this project to fruition. A preliminary engineering study at a cost of \$6,000 is complete. It shows the feasibility of this project and an estimate of the costs. The city is also willing to contribute its share with funds and services in kind. We urge you to support this proposal for the safety and benefit of Livingston's residents, businesses, and visitors.

Sincerely,

Jeanne-Marie Souvigney
Chair, Livingston Parks and Trails Committee

References:

- US Dept of Transportation: Shoulder Widths
http://safety.fhwa.dot.gov/geometric/pubs/mitigationstrategies/chapter3/3_shoulderwidth.cfm
- Livingston Quick Facts: Employers
http://www.livingstonmontana.org/living/quick_facts.html
- Yellowstone Bridge Project
<https://www.mdt.mt.gov/pubinvolv/yellowstone>
- Livingston Healthcare News
http://billingsgazette.com/news/state-and-regional/montana/new-m-hospital-set-to-open-in-livingston/article_b0da9fb7-f57a-506e-9193-a9af0a8863da.html



Park County Public Transportation Advisory Committee

February 3, 2017

c/o Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena MT, 59620-1001

Dear Mr. Holien,

It is my pleasure to send a letter in support of the City of Livingston's Grant Application to the Montana Transportation Alternatives Program for funding to complete a safe pedestrian and bike path as a connection to the Veteran's bridge and ultimately the hospital. As someone who frequently uses my bike to get around town, I have traveled to the hospital and felt quite unsafe. The proposed safety trail would be a grand welcome improvement.

I have a keen interest in the completion of this section of trail, for both safety and for promoting active transportation in our community. I currently serve on two volunteer citizen advisory boards. The first, is the Park County Transportation Advisory Committee (PCTAC) and our mission is to provide a comprehensive transit plan for Park County—starting with the city of Livingston, first. Our board is in support of promoting non-motorized transit opportunities, especially on the end of town where our hospital is located--both for workers and patients. The second is Angel Line Transportation whose purpose is to provide transportation to seniors over the age of 60 and the disabled in our community. We operate 3 small buses from our office in the Senior Center. The proposed path would make it so much safer when our buses head to LHC, which is often.

It is encouraging to see the progression in expansion activities of alternative transportation means in our rural community. I hope that you will consider this proposal favorably. Please contact me at 406-823-9503 should you have any questions.

Thank you for your consideration.

Sincerely,


Heidi Barrett
Chair, Park County Transportation Advisory Committee
Chair, Angel Line Transportation

231 S. H Street
Livingston, MT 59047



Park County Planning & Development Board
414 East Callender St.
Livingston, MT 59047

February 7, 2017

c/o Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena MT, 59620-1001

Dear Mr. Holien:

As Chairman of the Park County Planning & Development Board it is my pleasure to send a letter in support of the City of Livingston's Grant Application to the Montana Transportation Alternatives Program for funding assistance to further connectivity and safety in our vibrant, growing community.

This project has been long sought by our active community. It will not only enhance recreational opportunities through increasing connectivity for an existing trail system, it will also provide an alternative means for individuals to access the new hospital and has further implications for other development prospects on the east side of town. This is particularly important with respect to our organization in that our Board's chief concern deals with thoughtful planning for the long term betterment of our community.

It is encouraging to see the progression in expansion activities of alternative transportation means in our rural community. I hope that you will consider this proposal favorably. Please contact me at 406-581-0025 should you have any questions.

Sincerely,


Peter Fox
Board Chair



February 3, 2017

Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena MT, 59620-1001

Dear Mr. Holien:

On behalf of the Park County Health Department, I am pleased to submit this letter of support for the O Street-to-Bennett Multi-Use Path and Connector project application submitted by the City of Livingston.

The mission of the Park County Health Department is to promote and protect health while preventing disease through education, development and implementation of community health services in Park County. Responsibilities and services of the Park County Health Department include the promotion of the health of individuals and families in the County through the development and implementation of community health services. The Department focuses on the health of individuals through services of targeted public home health visitation, immunizations, communicable disease surveillance and prevention, public health emergency preparedness, senior health, family and child health, mental health and education outreach. The Health Department recently completed the 2015-2019 Strategic Plan with goals and objectives related to promotion of healthy behaviors. Connectivity advantages of the O Street-to-Bennett Multi-Use Path and Connector project will enhance and expand upon the many outdoor recreation amenities available in the City as well as provide a much needed safe pedestrian route to Livingston HealthCare.

The Park County Health Department looks forward to seeing the O Street-to-Bennett Multi-Use Path and Connector project in completion as it will be a huge community asset. If you have any questions about the Health Department, or its support for the O Street-to-Bennett Multi-Use Path and Connector project, feel free to contact me at 406-222-4140.

Sincerely,

A handwritten signature in black ink, appearing to read "Heather Jurvakainen".

Heather Jurvakainen, RN
Park County Director of Human Services



PARK COUNTY ENVIRONMENTAL COUNCIL

c/o Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena MT, 59620-1001

As a not-for profit organization dedicated to enhancing the quality of life in Park County, the Park County Environmental Council (PCEC) offers our enthusiastic support of the City of Livingston's Grant Application for funding assistance to improve walking and biking infrastructure in our community.

The proposed project would create a safe corridor to connect two popular recreation areas and the local hospital, offering endless opportunities for residents and tourists to get outside, enjoy exercise and outdoor recreational opportunities by the iconic Yellowstone River. On a personal note, as a parent of young children that enjoy riding bicycles, the proposed project is an absolute necessity – the current road has no shoulder and is incredibly dangerous for young riders. This project would be a great asset for cyclists of all ages.

Thank you for your consideration, we hope you consider the City of Livingston's Grant Application favorably. I encourage you to reach out to me via email erica@envirocouncil.org if you have any questions or concerns.

Sincerely,
Erica Lighthiser



Development Director
Park County Environmental Council

Livingston HealthCare
320 Alpenglow Lane
Livingston MT 59047

Feb. 3, 2017

Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PC Box 201001
Helena MT 59620-1001

Dear Mr. Holien,

I'm writing this letter in support of the City of Livingston's Grant Application to the Montana Transportation Alternatives Program for funding assistance to further connectivity and livability in our vibrant and scenic community.

This project has long been sought by our active community. It will not only enhance recreational opportunities; it will also provide an alternative means to access the hospital grounds and has further implications for development prospects on the eastern side of town.

The goals of City of Livingston's Grant Application to the Montana Transportation Alternatives Program clearly meet the priorities set forth in the MDOT application guidelines. Please consider full funding of this request. If you would like any more information regarding my support of the project, please contact (406) 823-6262.

Sincerely,
Jocelyn Stott
Marketing and Communications Manager

PARK COUNTY COMMUNITY FOUNDATION

February 3, 2017

Bruce C. McKnight, Chair
Paradise Valley, MT

Annie Beaver, Vice Chair-
Treasurer
Clyde Park, MT

Bob Hove, Secretary
Paradise Valley, MT

Michael P. Atkinson
Shields Valley

Amy Uberuaga Clark
Livingston

Donald B. Gimbel
Paradise Valley

Emily Post
Livingston

Carole Sullivan
Livingston, MT

Debra Swandal
Wilsall, MT

Jen Vermillion
Shields Valley, MT

Jeff Welch
Shields Valley

Peter D. Fox
Executive director
Shields Valley, MT

Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena, MT 59620-1001

Dear Mr. Holien:

On behalf of my colleagues of the Board of Directors of the Park County Community Foundation, I write to convey our strongest support for the City of Livingston's grant application to the Montana Transportation Alternatives Program for funding assistance to the O Street-to Bennett Multi-Use Path and Connector Project. We view this project as an essential public safety improvement along the busiest and most narrowly constricted travel corridor in the city and perhaps the entirety of Park County.

Each of us on the Community Foundation board have witnessed pedestrians and bicycle riders hazarding vehicular traffic on narrow Highway 89/East Park Street as they make their way between the safety of city sidewalks and wider roadways and the pedestrian/bike lane of Veterans Bridge over the Yellowstone River. Without this safety improvement, we fear it is only a matter of time before a child, teenager or adult is seriously or fatally injured only this constriction.

We also view this improvement resulting in improved alternate-transportation recreational opportunities for residents and visitors alike as they move between the city and the new Livingston HealthCare hospital campus and parkland to the east side of the Yellowstone. Residents have long advocated for this project, not only because of our active local lifestyle, but also because the land east of the river is identified for future residential and commercial growth.

We urge your support for full funding of this critically important initiative for the safety of Livingston and Park County residents and visitors, as well as an important component for future civic growth and economic development.

Sincerely,



Bruce C. McKnight
Chairman of the Board
Park County Community Foundation

"Connecting caring people and resources with community needs and opportunities."



February 14, 2017

c/o Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena MT, 59620-1001

Dear Dave,

As the Executive Director of the Shane Lalani Center for the Arts in Livingston, Montana, I am writing to express my support of the City of Livingston's grant application to the Montana Transportation Alternatives Program for funding to assist with connecting pedestrian routes in our community and improving safety along a very dangerous part of our main corridor into and out of town.

As a former employee of Livingston HealthCare, I have first hand experience with this route. As an active community member, I have run and biked this stretch of road. It is not a comfortable experience. Without the proposed connection, I fear that we are just waiting for a tragedy to occur. As the current executive director of the Shane Center, I am aware that we have many kids that bike from that side of town, across Park Street, to the Shane Center for practices, activities, classes, and camps. I would love to see this be a safer journey.

Thank you for your consideration. The City of Livingston's Grant Application to the Montana Transportation Alternatives Program carries our full support. We hope that you consider their application favorably. Please contact me at (406)222-1420 if you have any questions regarding our support.

Best regards,

Sally O'Connor
Executive Director
The Shane Lalani Center for the Arts



PO Box 58 Livingston, MT 59047 www.theshanecenter.org



(406) 222-1420



"Strengthening community through participation in the arts"

Jimmy Udall
426 S. 8th Street
Livingston, MT 59047

Jimmy Udall January 24, 2017

Dave Holien
Acting TA Program Manager
Montana Department of Transportation
PO Box 201001
Helena MT, 59620-1001

Dear Dave Holien:

As an avid bike commuter and Livingston Healthcare employee I'm writing to express support for the City of Livingston's Grant Application to the Montana Transportation Alternatives Program for funding to assist with connecting the hospital to downtown through a bike path.

The proposed bike path is an essential link for commuters and patients who would visit the hospital. Even as a seasoned bike racer and longtime bike commuter, I find the current commuting options grim and dangerous. In addition to the safety of our many patients, employees and visitors, that section of town is a gateway to recreational biking on the north side of town and a connector would add to Livingston's livability by adding recreational opportunities for our active residents.

Thanks for considering this proposal. Livingston is increasingly populated by people looking for safe commuting options. With our (relatively) warm weather we're just a few safe connector trails away from being one of the most bike friendly small towns in Montana. Please feel free to contact me at (406) 823-6650 if you have any questions about my support.

Sincerely,

Jimmy Udall

January 30, 2017

c/o Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena MT, 59620-1001

Dear Mr. Holien,

As a dietitian working for Livingston HealthCare in Livingston, Montana, I am writing to express my support of the City of Livingston's Grant Application to the Montana Transportation Alternatives Program for funding to assist with connecting our community and improving safety along a particularly dangerous portion of our main corridor. I ride my bike to work after dropping off my children at day care, my bike has a kid cart attached to the back and it makes me very nervous to ride on the stretch between O Street and the Veterans Bridge. I will not ride that stretch with my children in tow as it is now. I also manage the Café within Livingston HealthCare and I would like to encourage people to ride or run or walk out to our facility to enjoy our patio via the new walking bath on the east side of Veterans Bridge. Without a shoulder between O Street and the bridge, I think it hinders travelers from walking/running/riding our way. This is unfortunate since being active is something we as a community should foster.

It is my pleasure to send a letter in support of the City of Livingston's Grant Application to the Montana Transportation Alternatives Program for funding assistance to further connectivity and livability in our vibrant and scenic community. I think the Trails Rx program has done a wonderful job creating new trails to encourage safe places to exercise; continuing with this shoulder from O Street to the bridge will only strengthen these projects.

Thank you very much for your consideration. I believe this project will help to create an environment of Wellness as we make alternative transportation safer in our community.

Please do not hesitate to contact me with questions.

Sincerely,



Jessica Wilcox MS, RD, LN

(406)570-0377

Jewilliams2010@gmail.com

February 1, 2017

David Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena MT 59620-1001

Mr. Holien,

Please accept this letter in support of the City of Livingston's Grant Application to the Montana Transportation Alternatives Program. We have a bike/pedestrian route in Livingston that is in desperate need of a safety upgrade due to poor visibility, high traffic, non-existent shoulder along a state highway, and a very steep drop off.

Once accomplished, the *O Street-to-Bennett Multi-Use Path and Connector Project* will provide an important alternative transportation route for bike and pedestrian traffic. It is the final section of a route that will connect our downtown and multiple residential neighborhoods with our new Critical Access Hospital and a beautiful public hiking/running trail system along the Yellowstone River.

As a citizen of Livingston and a physician the hospital, I have ridden this path in its current condition many times, both on my way to and from work, as well as to recreate with my family. As traffic increased over the summer and the days shortened this fall, it became too dangerous to continue. Many of my co-workers, patients, visitors to the hospital, and friends have also experienced dangerous moments on the path. All were reluctant to continue using it in its current condition. Unfortunately, there is no other access to the hospital or the river park other than by private vehicle.

Several factors locally increase the need and significance of this proposed project. For example, we have a large number of low-to-moderate income citizens in Livingston (~45%) who have very limited transportation options. These individuals are dependent on safe walking paths or biking routes to key locations like the hospital /clinic/ER. Further, being overweight and obese are also issues in our community, with the adult obesity rate at 23% in Park County, according to the Robert Wood Johnson Foundation; this number was found to correlate to decreased access to trails and exercise. Thus, active transportation routes will offer both quality of life and health benefits. Overall, access to recreation areas close to town will benefit children, pet owners, families, and visitors alike.

As a primary care physician with expertise in population health, I am keenly aware of the need for safe and convenient access to healthcare and recreational areas. Both are crucial aspects of healthy, thriving, and livable communities. The funds requested to support the "O Street-to-Bennett Multi-Use Path and Connector Project" will benefit a large number of people from all walks of life, and strengthen our community's commitment to active transportation, healthy, and safe living. I hope that you will agree and offer the full funding of this request. If you would like any more information regarding my support of this project, please do not hesitate to contact me at 406-220-0213 or by email at ldesnick@uw.edu.

Sincerely,
Laurel Desnick, MD
406-220-0213
Livingston Healthcare
University of Washington School of Medicine



Rx Coffee
1404 East Park Street
Livingston, MT 59047

February 14, 2017

c/o Mr. Dave Holien
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
P. O. Box 201001
Helena, MT 56920-1001

RE: Montana Transportation Alternatives Program

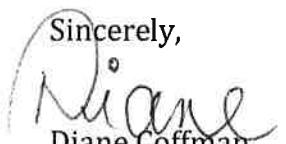
Dear Sir or Madam,

As an business owner ajacent to the proposed site in Livingston, Montana, I am writing to express my support of the City's Grant Application to the Montana Department of Transportation Alternatives Program for funding to assist with connecting our community and improving safety along a particularly dangerous portion of our main corridor.

Having experienced this narrow shoulder firsthand, the proposed, and long overdue, multi-use path will create a safe route for bicycles and pedestrians to enhance multi-modal transportation and recreational opportunities. Further, it will provide an added means for individuals to access the new hospital and has greater implications for development prospects on the east side of town. This is particularly important with respect to my coffee shop business and the bus stop. People will take this path for exercise and will likely take a break at my shop and treat themselves to a coffee drink, thus allowing for economic opportunities and growth. Once the proposed project is approved, I anticipate that it will allow me to extend my business hours. In addition to economic implications, there are also serious safety implications to consider. Without the proposed connection, I suspect it will just be a matter of time before there is a tragic fatality on this road with the bicyclists and/or pedestrians.

The goals of the City of Livingston's Grant Application to the Montana Transportation Alternatives Program clearly meet the priorities set forth in the MDOT application guidelines. I hope that you will consider full funding of this request. If you would like any more information regarding my support of this project, please contact me at 406-223-6886.

Sincerely,


Diane Coffman
Owner, Rx Coffee

c/o Mr. Dave Holien Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena MT 59620-1001

02/16/2017

Dear Sir or Madam,

Thank you for taking time to read my Letter of Support for the City of Livingston's request for funding to help with our Multi-Use Path. My name is Chris Benden. My wife and I own the majority of the property along the pathway which is proposed. To have a safe route for walkers and bikers alongside this ever popular and dangerous portion of roadway would be a most welcome achievement.

For years and years, this stretch of road from the old highway bridge over the Yellowstone River to 'O' street, or the town's edge, was used mainly by persons without transportation and only the occasional bicyclist. Now this didn't mean it was safer, just less traveled. Well, we have a whole new game in town these last five years. First the incinerator was closed. All the refuse and recycling was moved much closer to the area proposed for the new pathway - to the City's Transfer Station on Bennett Street. This means more traffic on the stretch of road where the new pathway is planned. Next, the city developed some park space and recreational land on the east side of the river. This land is located at the east end of the proposed pathway. Next, a new bridge, now called the Veteran's Bridge, was erected with a built in foot and bike path. With this much wider bridge came more traffic. I know of multiple persons who avoided the previous bridge due to its narrowness. Finally, came the new hospital, which is located east of the proposed pathway and connects to the previously mentioned city recreational area. Add up the increased motor traffic along with more and more bicyclists, joggers, walkers etc., we now have increased odds of pedestrian-vehicle incidents.

I probably have witnessed more foot and bicycle traffic along this stretch than anyone. I get very nervous when I see kids, dog walkers, elderly all trying to get down a road with a two foot or less shoulder. What a great thing it would be for people to access this east end of town without fear. Your grant funding will help us improve our corridor safety, and it has the full support of my wife and me.

Thank you,
Julie and Chris Benden
Adjacent Property Owners



WILL ERASE FOR SAFER

Dear Mr. Dave Horan,
I am writing to you to
convey my support for the

"O Street - to - Bennett Motor -

use Park and Connector Project"
located in Livingston.

AUKE.

WE ALSO ENCOURAGE
THE CONNECTOR TRAIL

I AM A FREQUENT USER
OF THE MEADS VIEW TRAIL.
IN FACT, I FIRST USED IT
THIRTY YEARS AGO &
WANT TO START MY DAY

FURTHER BENEFITS PROVIDED
ACTIVITY FOR LOCAL &
VISITORS TO LIVINGSTON.

THAT'S WHY FOR YOUR
TIME & HOPES YOU SUPPORT
THEIR PROJECT.

THEIR "CONNECTOR TRAIL"

Sincerely, Tim Horan

From: David Pettit
Date: January 23, 2017
To: Montana Transportation Alternatives

To Whom it may concern,

As a science teacher and an outdoor enthusiast in Livingston, Montana, I am writing to express my support of the City of Livingston's Grant Application to the Montana Transportation Alternatives Program for funding to assist with connecting our community and improving safety along a particularly dangerous portion of our main corridor.

This project has been long sought by our active community. It will not only enhance recreational opportunities, it will also provide an alternative means for individuals to access the new hospital and has further implications for other development prospects on the east side of town. This is important to me as a science teacher (SAFETY, SAFETY, SAFETY) because my classes travel this area on various field trips. (We ride our bikes during class; to EXPLORE, and study the ecology of the area.)

Thank you, for your consideration of this project. I can and would love to give further information. I can be reached at (cell: 1-406-220-1082) E-mail: dpettit@livingston.k12.mt.us.

David Pettit
1114 Ridgeview Trail
Livingston, Montana 59047

February 6, 2016

c/o Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena MT, 59620-1001

Dear Mr. Holien,

As an active and involved resident of Livingston, MT, I'd like to voice my strong support for the **O Street-to-Bennett Multi-Use Path and Connector Project**. I am a board member of the Livingston Urban Renewal Agency and an advocate for trail connector paths to our city parks and trails. My family and I have lived on L St., a few short blocks from the proposed O St. connector path, for 6 years now.

Livingston is a very young and active community. We are fortunate to live in a condensed yet rural community with many parks and trails throughout town. However, we have a strong need for connector trails. If there is an employer, park or fishing access within walking or biking distance of our homes, the citizens prefer to forgo their vehicle and walk, jog or bike to said destination. I can only imagine how many hospital employees prefer to commute to the hospital on their bikes.

Since the completion of the Veteran's Bridge, I have long felt there should be safe public pedestrian access from O St. to the Bridge, connecting Livingston and its East side community to the beautiful Myers Park, Yellowstone River and the new hospital. For example, last summer I passed two people walking on the 150 yard section of the highway that is in question. One was a middle aged man in fitness clothing, walking towards town from the bridge, and the other was a young boy about 12 or 13 yrs. walking towards the bridge with a fishing pole and tackle box. Both were walking on the same side of the highway as the pedestrian side of the bridge, which has a very narrow shoulder. Also, my husband and myself routinely use this access to the Veteran's Bridge and take our lives into our hands while running from O. St. to the Bridge along the highway. Every time I run on that section, I worry someone might hit me. I also worry about other pedestrians—especially a child/youth—getting hurt or worse, while trying to walk, bike or run to the city park, river or the hospital.

It is encouraging to see the progression in expansion activities of alternative transportation means in our rural community. I hope that you will consider this proposal favorably. Please contact me at 406-223-3959 should you have any questions.

Sincerely,

Kyra Ames
Parent/out-my-front-door enthusiast/Urban Renewal Agency board member

January 24, 2017

Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
P O Box 201001
Helena, MT 59620-1001

Re: Livingston "O" St. Extension Proposal

Dear Mr. Holien,

I am a citizen who likes to bike, and I have had the scary experience of biking on East Park St. in Livingston, between O St. and the Veteran's bridge. This is the local truck route so when the I-90 Highway is closed due to high winds, all east bound and west bound traffic uses this piece of Business 90. It is a very uncomfortable experience, feeling very vulnerable to a slight mis-judgment by a truck driver that might run into me. In high wind conditions, all traffic such as over-filled hay wagons, semi-truck trailers, (often with 2 trailers), flatbed 3500's, dump trucks, vehicles hauling trash and yard waste to the adjacent drop off site and the usual vehicular traffic make a steady stream of threat to bicyclists or pedestrians on the almost non-existent paved shoulder.

A local proposal intends to alleviate that. Our new Livingston Healthcare Hospital is across the river and has created an on-premises trail. From that trail, bikers or pedestrians can reach another piece of trail on the east side of the Yellowstone River. The new Veterans Bridge across the river connects the 2 eastern trails to the city of Livingston. A widened O St. to Bennett St. multi-use pathway will connect all these parts and allow for safer travel for recreationists, children, and bike commuters who work at the hospital. Trail connectivity is essential for ease of use and to cross a distance for work or play. The best, safest trails keep pedestrians, bikers, skaters, baby strollers, etc. away from vehicular traffic. This trail extension is a step toward that goal.

I support the City of Livingston's grant application to the Montana Transportation Alternatives program to fund this connection allowing improved safety for non-motorized users. It is encouraging to see the support for alternative means of moving into and through the community via surface trails. Please agree by funding the O St. to Bennett St. Multi-use Path and Connector Project. Thank you for reading.

Sincerely yours,


Jerry Ladewig
P O Box 1184
Emigrant, MT 59027

Suzanne Goodman
204 South K Street
Livingston, MT 59047

January 26, 2017

Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena, MT 59620-1001

Re: *Livingston's O Street-to-Bennett Multi-Use Path and Connector Project*

Dear Sir or Madam,

Thank you for considering Livingston's grant proposal to the Montana Transportation Alternatives Program to help construct a safe pathway from O Street to Veterans Bridge. The trail system east of the bridge is an asset to Livingston. An easy and safe way to access the trails from town is mandatory, as well as to access the hospital through means other than motorized vehicles. Without a safe connection, as proposed by the City, the beautiful pedestrian lane on our new bridge is pointless and seems a shame to our veterans after whom the bridge is named. With a safe route from O Street, Livingston residents and visitors could not only safely access the trail system and hospital, but could easily enjoy a nice long walk or ride around Sacajawea Park to Mojo Park, pass through some quiet east side neighborhoods, and emerge at the bridge and continuing on to the trail system. That would be wonderful!

I frequently take walks in Livingston. I enjoy the trails near the hospital and the trailside attractions. That narrow stretch of highway, however, where there is no shoulder and the roadside drops off at a frightening angle, keeps me from heading to those trails very frequently. I fear it is only a matter of time before an unaware motorist either passes too closely to me or swerves at the wrong time too far into oncoming traffic. It is ironic that trails designed to help us stay healthy, also provide an opportunity to put our lives in danger.

Again, the City of Livingston's Grant Application to the Montana Transportation Alternatives Program carries my full support. I hope that you consider their application favorably.

Sincerely,

Suzanne Goodman

Rose Brown

411 S K St.
Livingston, Mont.
59047
Rose.gertrude.brown@gmail.com

January 23, 2017

Dave Holien
Acting TA Program Manager Montana Department of Transportation Engineering
Division/Consultant Design Bureau
PO Box 201001
Helena MT, 59620-1001

Dear Mr. Holien,

Thank you for considering construction of a safe pedestrian/bike path between the East edge of Livingston and the Veterans Bridge.

I am a full-time mom, and my family and I love to enjoy Livingston's Parks and Trail Systems. After the Veterans Bridge was completed, we thought it would be fun to take a family bike ride out to the Myers Riverside Trail Complex with my toddler in tow in a Chariot Trailer pulled behind my bike. When we arrived as far as the bus station we immediately observed that it would be treacherous to ride the distance from town to the bridge, so we turned around and rode somewhere else. It seems a waste to have this great recreation area inaccessible to bikers and biking families because of such a short section of impassable highway.

A safe pedestrian/bike path here would also be really practical for bikers and pedestrians wanting to travel from one side of town to the other via the Bennett Street railroad crossing, whether for work or recreation. Without a designated path, this section of road is not safe, especially if you have a toddler trailer in tow, which is a very common way for young families to get around.

Sincerely,

Rose Brown

Dave Holien
Acting TA Program Manager
Montana Dept. of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena, MT 59620-1001

Hi:

I want to express my strong support for a pathway to be built from O-Street to Bennett in Livingston.

I think it's critical to expand our trail system for pedestrians and bicyclists. I believe doing so is good for our economy (attracts more residents and vacationers) and protects and promotes health and safety.

As Livingston grows, such as with the addition of the new hospital, I believe that providing alternative options for transportation is a key part of this development. Studies have shown that sidewalks, pathways and trails are beneficial to communities.

I know that I, as a Park County resident who is in Livingston frequently, would take advantage of a growing trail system.

I ask that you please award a grant to fund this proposed project.

Thank you,

Wendy Riley

PO Box 566
Emigrant, MT 59027
wriley406@yahoo.com

BILLINGS MT 591

02 FEB 2017 PM 1 T

Karen Kaminski
703 W Summit St
Livingston, MT 59047



David Holien
active AT Program Mgr.
MT Dept. of Transportation
Engineering Division Consultant
P O Box 20100
Helena, MT 59620-1000

Please make a trail from O
Street to Veterans Bridge
I sometimes walk my dog (on
leash) there & the trip is
very dangerous

Karen Kaminski

Mr. Dave Holien

Acting AT Program Manager

Re: Urgent Livingston Parks and Trail Connector Path

Dear Mr. Holien:

I often times walk, since I walk most everywhere, the route to the beautiful new Veteran's Bridge over the Yellowstone River. There I have seen many kid's bicycles who have made their way to the River to fish and explore and have seen Livingston HealthCare workers making their way walking back into town. I have shuddered thinking of how they have traveled to that point.

Imagine your choice as you make your way along the busiest Highway route through the City of Livingston: 1) You go on 2 feet of pavement where on the left a five inch white line "protects" you from city traffic zooming by, or 2) You head down the steep 45 degree or so embankment for 15 yards and beat your way through weeds, deer remains, beer cans and other garbage before you "sneak" across some private driveways.

The connector route would immensely enhance existing trails already completed by the community and obviously enhance safety. The project merits your most appropriate consideration and would reflect very well upon your program.

Very truly yours,

Bob Jovick

410 S 8th Street, Livingston, Mt.

Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena MT, 59620-1001

February 2, 2017

John Greene
Livingston Bike Club President
307 S. 10th St.
Livingston, Montana 59047

Re: O Street-to-Bennett Multi-Use Path and Connector Project

Dear Mr. Holien,

I am writing in support of a safe path to the park across the river via Veteran's Bridge, the Meyer's River View Trail and Park. This area is a great asset to the community. A safe path and walkway would encourage its use by all citizens. Also, it would great for employees of the hospital.

The narrow section of US 89 that it would parallel is very dangerous. I personally had a close call when the carnival was in town and several trucks with various pieces sticking out passed each other on that road. In places there is no shoulder on this road. As such, this project would greatly improve safety and overcome these concerns.

In summary, the City of Livingston's Grant Application to the Montana Transportation Alternatives Program carries my full support. I hope that you consider their application favorably. Please contact me at 406-222-3562 if you have any questions regarding my/our support.

Sincerely,
John Greene



February 5, 2017

c/o Dave Holien, Acting TA Program Manager
Montana Department of Transportation
Engineering Division/Consultant Design Bureau
PO Box 201001
Helena MT, 59620-1001

As the president of Livingston Youth Soccer Association, as a father of teenage kids and as a regular exerciser; I am writing to express my support for the City of Livingston's Grant Application to the Montana Transportation Alternatives Program for funding to assist with connecting our community and improving safety along a particularly dangerous portion of our main corridor.

I have lived in Livingston for 18 years. I run and ride my bike around town year round, as do my kids. The proposed multi-use connector path is a much needed path that will safely connect multiple areas of town through a currently unsafe portion of Park Street.

Thank you for you considering this proposal. Don't hesitate to contact me if you have any questions.

John Gregory



President

Livingston Youth Soccer Association
126 South 6th St.
Livingston, MT 59047
406-220-2083

Jeanne-Marie Souvigney
212 South 6th Street
Livingston, MT 59047

406-581-8942
jsouvigney@onemain.com

January 31, 2017

Dave Holien
Acting TA Program Manager
MT Dept. of Transportation
Engineering Division/Consultant Design Bureau
P.O. Box 201001
Helena, MT 59620-1001

Dear Mr. Holien,

As a resident of Livingston, I am pleased to send this letter of support for Livingston's grant application to the MT Transportation Alternatives Program to help make our main route through town a much safer route for pedestrians, bicyclists, and motorists.

For several years, I have felt that making this section of road safer should be a priority. I often bicycle around town and have found this stretch of Park Street to be very unsafe travelling for bicyclists and pedestrians. There is simply no shoulder to ride on. Crossing back and forth on Park Street is not a viable option, as it puts one in the path of cars again and again, and the north side of Park Street doesn't have a travel shoulder, either.

Elderly people as well as mothers with small children struggle to navigate this corridor. With the new hospital, two recently established and well-visited trails and new businesses farther to the east, even more people are put at significant risk travelling this road. When traffic is routed through town because of high winds, the road is even more crowded and dangerous.

I currently serve as the chair of the City's Parks & Trails Committee, and our committee identified this shoulder widening and crosswalks as a priority a few years ago. (A separate letter of support has been submitted on behalf of the committee.) I also served on a joint Livingston-Park County task force to develop a trails plan for the city and the area immediately surrounding (2005-2007). The resulting Livingston/Park County Trails and Greenways Plan identifies the public interest in a safe passage along Park Street, across the Yellowstone River and east and north.

This route is a vital part of a viable, safe motorized and non-motorized travel plan for our community. We need to continue to improve the connectivity, safety and usefulness of our streets and trails so that we can meet the needs of all members of our community regardless of their age, physical condition, purpose or mode of travel. I believe that doing so will improve the health and quality of life of our all our residents and strongly support full funding of this application. I urge your support.

Sincerely,


Jeanne-Marie Souvigney

Appendix I

Livingston Enterprise

Related Local Newspaper Articles

Published Friday July 8, 2016

Let's act before there's a tragic bike accident on route to hospital

Editor:

It was with great sadness that I read about the accident in Park City Wednesday which took the life of a grandfather accompanying his grandson to school by bicycle.

I, along with many other employees, visitors and patients, ride my bike most days to the new hospital. Every time, I cross my fingers and hold my breath hoping to avoid being hit by a car.

A serious bike/vehicle accident seems inevitable given the current stretch from the Greyhound Station to the old KPRK building. Along that route there is no shoulder, no slow speed limit, no visibility as drivers face into the sun in the early morning hours, and a steep drop-off next to the road. Cars pass by inches from the edge of the road, and an accident seems only a matter of time.

It is urgent that the city, county and hospital get together to find a solution before we have a tragedy like Park City.

*Laurel Desnick
Livingston*

Tuesday, January 24, 2017

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City seeks grant for path from O street to Veterans Bridge

Published by Enterprise Staff on Tue, 01/24/2017 - 1:44pm



The City of Livingston is seeking funds to construct a pathway from the intersection of East Park and O streets to Veterans Bridge. (Enterprise photo by Hunter D'Antuono)

By: Liz Kearney
Enterprise Staff Writer

Livingston officials are looking for some financial help to build a safer walkway from the intersection of East Park and O streets to Veterans Bridge.

The O Street-to-Bennett Multi-Use Path and Connector Project includes plans for a bike and pedestrian path along East Park Street that would include a separation between the roadway and the path. The path would include a sidewalk that would wrap around O Street to East Callender Street. Signage would direct pedestrians and bicyclists traveling west to turn south onto O Street and continue on East Callender Street, rather than continuing onward on the busier East Park Street, Melody Montgomery the city's grant writer, said Monday.

Montgomery is working on a grant application to fund the project. The grant will be submitted to the Montana Department of Transportation's Transportation Alternatives program.

<http://www.livingstonenterprise.com/content/city-seeks-grant-path-o-street-veterans-bridge>

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The application is due March 31. The program is competitive, and about 25 to 30 percent of applications are funded, she said. If the grant is awarded, funds would be available in 2020, she said. And while it's early to estimate construction costs, Montgomery said the project might range between \$100,000 and \$200,000.

The city's Parks and Trails Committee recently identified a connector trail from South O Street to Veterans Bridge as one of its new trail priorities according to a committee report, and city officials support the idea.

"The city administration is and has been very supportive of the efforts to secure funding for this important project," Interim City Manager Lisa Lowy said Tuesday. "The goal of enhancing and further connecting our trail system benefits all of our citizens both in the city and county." The length of the proposed path is less than a mile, but it's an important trail connector, Montgomery said.

People will be able to travel from Mayor's Landing and connect with the Meyer's View Trail. The Meyer's View Trail connects to other trails, including those that travel to Livingston HealthCare. The trail connector will allow bicyclists and pedestrians — which includes hospital staff as well as hospital clients — to more safely travel to LHC.

The improvements would increase safety not only for bicyclists and pedestrians, but also for motorists, Montgomery said. She referred to a letter Laurel Desnick, a doctor at LHC, wrote to the editor of the Livingston Enterprise in July. Desnick wrote that she rides her bike to work most days, as do other staff, visitors and patients.

"Every time, I cross my fingers and hold my breath hoping to avoid being hit by a car," she wrote.

The roadway from O Street to the old KPRK building, Desnick wrote, has no shoulder, no slow speed limit, a steep drop-off, and drivers are facing into the sun eastbound in the morning. Cars pass by only inches from the edge of the road.

Montgomery is soliciting letters of support from community members to submit with the application.

"Lots of favorable letters came back quickly," she said, and her feeling is that many people support the project.

Support letters are needed by Feb. 7. Letters should be addressed to Dave Holien, Acting TA Program Manager, Montana Department of Transportation, Engineering Division/Consultant Design Bureau, PO Box 201001, Helena, MT, 59620-1001. The letters, in PDF format, should be emailed to Montgomery at mmontgomery@livingstonmontana.org.

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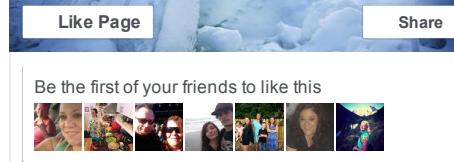
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IN THE MAIL

Don't keep Growth Policy process in the shadows

Editor:

The deadline for commenting on the Park County Growth Policy is approaching. Unfortunately, if you read the county website, you cannot determine when the comment period will end or when the vote will take place.

The purpose of this portion of the process is to provide comments to the County Commission. The website directs comments to the Planning Department. The Planning Board already has reviewed the document and provided comments. All comments should be submitted to the commission if they are following the proper procedure.

The website encourages citizens to become involved and states that copies are available in various locations throughout the county. However, there is only one copy at the location I checked. At that site, a cheap clip is holding the 80 pages together.

The commission needs to do a better job of informing the public and not keep the Growth Policy process in the shadows.

Marty Malone
Pray

Montanans must again stop Gianforte

Editor:

With Ryan Zinke expected to be confirmed as the secretary of the Interior Department, Montanans will soon have a unique opportunity to choose a new representative in the U.S. Congress.

Though the candidates have yet to be announced, Greg Gianforte — a multimillionaire from New Jersey whom Montanans rejected as their governor a mere two months ago — is expected to be the nominee for the Republican ticket.

Gianforte seems to think Montanans have a short memory. But we do not. We remember his documented attempt to block ordinary Montanans from accessing a public easement through his personal property. We remember his support of levying a state sales tax that would benefit large corporations while hurting low-income families. We remember the misrepresentations and outright lies he advanced during the 2016 election, including one assertion directly debunked by executives at Facebook. We remember his long history of donating millions to antiscience organizations and ongoing advancement of a religious ideology far more radi-

cal than that of ordinary churchgoing Montanans.

When Montanans go to the polls in this special election, it is our duty to look past the "R" and "D" behind the candidates' names and instead truly contemplate which person has our best interests at heart. And for the second time, we must send a message to Gianforte — we must tell him that Montanans will not allow a power-hungry outsider with deep pockets to use our great state as a political springboard to advance his own ego and self-interested agenda.

Amy Katherine Talcott
Livingston

Thanks to city on path work

Editor:

On behalf of Livingston's Parks and Trails Committee, I would like to extend sincere thanks and gratitude to the city for its work to secure funding for the O Street to Veterans Bridge shoulder widening, which is a priority for our committee. As reported recently in The Enterprise, the O Street-to-Bennett Multi-Use Path and Connector Project, as it's formally called, will provide a safe bike and walking lane from downtown Livingston over Veterans Bridge to Livingston HealthCare, the broader trail system at Meyers' River View Trail and Alpenglow Trail and businesses to the east.

We, as a committee, are motivated by our belief that enhancing Livingston's parks and trails make our community a better place to live. This project vastly improves the safety for motorists, bicyclists and pedestrians who regularly use this stretch of road.

This project has been a top priority of Livingston's Parks and Trails Committee for several years. Melody Montgomery, the city's grant writer, as well as Shannon Holmes, Livingston's director of Public Works, and Lisa Lowy, interim city manager, have been instrumental in helping to advance this vitally important undertaking, as have key Park County committee members.

The grant being submitted by the city is extremely competitive. We urge residents from Livingston and across Park County to write letters of support. We are confident that with overwhelming public support and a strong application, the Montana Department of Transportation's Transportation Alternatives grant program will choose to fund this project. All letters should be addressed to Dave Holien, Acting AT Program Manager, and emailed to Melody Montgomery at mmontgomery@livingstonmontana.org by Feb. 7. You can also contact Melody or me for more information.

Jeanne-Marie Souvigney, Chair
Livingston Parks and Trails Committee

Appendix J

MDT ROW Delineation

May 2016

O Street Path R/W delineation

(MDT May 2016)



Appendix K

Transportation Impact Fees *Excerpt - pg. 117 of CIP*

http://www.livingstonmontana.org/living/docs/Finance/2017_2021_CIP_Electronic_.pdf

SPECIAL REVENUE FUNDS IMPACT FEES FINANCIAL ANALYSIS

CAPITAL IMPROVEMENT PLAN TRANSPORTATION IMPACT FEES - (FUND 2399) Fiscal Years 2017 - 2021						
	Year 1 FY 2017	Year 2 FY 2018	Year 3 FY 2019	Year 4 FY 2020	Year 5 FY 2021	
Estimated Beginning Fund Balance	\$ 217,898	\$ 158,298	\$ 129,106	\$ 100,330	\$ 71,979	
Add:						
Operating Revenues	20,400	20,808	21,224	21,649	22,082	
Transfers In	-	-	-	-	-	
Total Revenues	20,400	20,808	21,224	21,649	22,082	
Subtract:						
Operating Expenditures	-	-	-	-	-	
Capital Expenditures	80,000	50,000	50,000	50,000	50,000	
Transfers Out	-	-	-	-	-	
Total Expenditures	80,000	50,000	50,000	50,000	50,000	
Estimated Ending Balance	\$ 158,298	\$ 129,106	\$ 100,330	\$ 71,979	\$ 44,060	

CAPITAL IMPROVEMENT PLAN TRANSPORTATION IMPACT FEES - (FUND 2399) Fiscal Years 2017 - 2021						
	Year 1 FY 2017	Year 2 FY 2018	Year 3 FY 2019	Year 4 FY 2020	Year 5 FY 2021	

Equipment

\$ -

Projects

Pavement Projects <i>(Pavement of Gravel Streets)</i>	30,000	30,000	30,000	30,000	30,000	150,000
ADA Accessibility Upgrade	10,000	10,000	10,000	10,000	10,000	50,000
Underpass Design						- TBD
Storm Drain Feasibility Study	30,000					30,000
Grant Cash Match	10,000	10,000	10,000	10,000	10,000	50,000

Total	80,000	50,000	50,000	50,000	50,000	280,000	
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